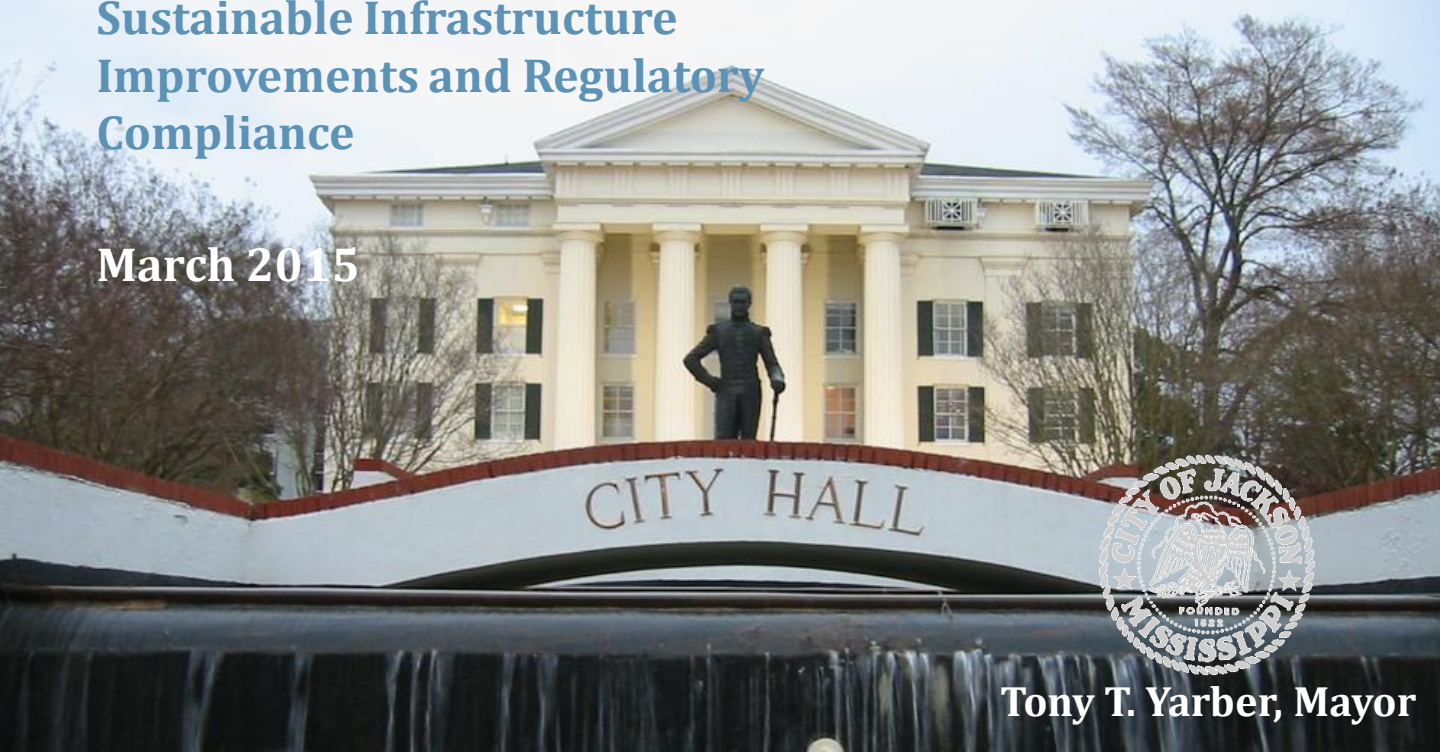




The Bold New Infrastructure Improvement Plan

An Integrated Plan for
Sustainable Infrastructure
Improvements and Regulatory
Compliance

March 2015



Tony T. Yarber, Mayor

The Bold New Infrastructure Improvement Plan:

An Integrated Plan for Sustainable Infrastructure Improvements and Regulatory Compliance



City of Jackson
Department of Public Works

Kishia L. Powell, PE
Director

March 2015

From the Mayor

Greetings,

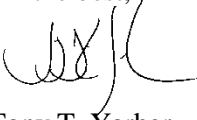
On January 14, 2014, citizens from all over our great city convincingly passed legislation to approve a 1% sales tax increase to address our infrastructure needs. Infrastructure stability is a prime concern across our country. With a huge disparity in the amount of funds needed versus what can be allocated, infrastructure maintenance and stability are being reevaluated nationwide. Our infrastructure concerns are no different from other cities. Yet, what we've done here in Jackson is take a progressive approach to address the issues that have plagued our city for decades. This approach has put us in the front seat of a national conversation.

In April of 2014, this administration took on the challenge to bring about positive change in Jackson and to provide the best possible quality of life for all its constituents. We saw the conversation on sustainable infrastructure management as an opportunity to solidify Jackson as a leader in innovation and planning throughout our great nation. After assuming office in April, this administration immediately rolled out initiatives – such as our infrastructure listening tours - to involve the community in the process of planning for our infrastructure improvements. Using feedback from those listening tours, direction from subject matter experts, and input from the commission set in place to oversee the 1% sales tax funds; this administration has crafted and proudly presents the **Bold New Infrastructure Improvement Program for Jackson, MS: The Bold New City.**

The vision of our city can only be accomplished through our collective genius and collective effort. This administration submits to you that at every level, in every department, in all tasks – we will be committed to our citizens, innovative in our thoughts, seamlessly connected in our processes, champions of our vibrant city, diligent in our efforts to assure the progression of our people, and a prosperous city with leaders that exemplify the level of excellence that Jackson deserves. As we push forward our Bold New Infrastructure Improvement Program, I thank you for the confidence that you have placed in me to lead this city toward the manifestation of its potential.

We are Vibrant! We are Connected! We are Prosperous! We are Innovative! We are Committed!
We are Jackson!

All the best,



Tony T. Yarber
Mayor of Jackson

Delivering the Vision: *A Bold New City*

Transforming the City of Jackson to become a destination city that is a leader in innovation, education, infrastructure stability; cultivating an environment that is safer and conducive to entrepreneurial endeavors.



MAYOR
TONY YARBER

City Council

President

De'Keither Stamps, Ward 4

Vice President

Melvin Priester, Jr., Ward 2

Council Members

Ashby Foote, Ward 1

Kenneth Stokes, Ward 3

Charles Tillman, Ward 5

Tyrone Hendrix, Ward 6

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Executive Summary

Over the last several years the City of Jackson has spent hundreds of thousands of dollars on street resurfacing only to have shifting soils and an escalating number of utility cuts to return our roadways to an obstacle course of potholes. State Street, a major gateway into our city was resurfaced in 2010 with 26 utility cuts since then; a sign of our City's failing water system that is exhibiting an estimated 40% water loss and whose loss of reliability has sent some of our largest users to private wells.

In March of 2013, the City received a federally mandated wastewater consent decree as a result of hundreds of sanitary sewer overflows (SSOs) and prohibited by-passes from the wastewater treatment plant. Add to this severe drainage and flooding issues that have caused significant impacts to public and private property and a system of bridges that have ratings as low as a 9 on a 100 point scale; it's no doubt the City of Jackson is in need of an infrastructure overhaul.

Infrastructure Improvement Need

The national conversation tells us that public infrastructure assets are in significant need of repairs and upgrades just to maintain current service levels but there is a severe shortage of capital; in Jackson, the conversation doesn't change. We have to do more, from years of deferred maintenance, with less. **Our total estimated needed infrastructure spending in Jackson exceeds \$743 million between now and 2031 of which \$664 million is not yet funded**, and it is critically important to note that these estimates are based on what we know about.

Assets	Metrics	Identified Investment Need	Investment Need Basis
Streets	1200 maintained miles of streets (2,055 lane miles)	Roadway resurfacing (milling and overlay) needs are currently estimated at \$44.5M	149.59 lane miles of milling and overlay only; does not include reconstruction or other improvements
Bridges	200+ bridges and hydraulic structures	MDOT State Aid has evaluated the City's 200+ bridge structures and assigned 60% of the structures a sufficiency rating of 50 or less	Not estimated
Drainage	Miles of drainage ways unknown	Drainage improvement needs are currently estimated at \$40.1M	25 locations with severe drainage issues
Water	1100 miles of water main; Two Water Treatment Facilities; 40% system water loss	Water infrastructure improvements is estimated at \$248.3M	8% of the system is over 100 years old; 97 miles of 2 to 4-inch pipe needing upgrades

Wastewater	1000 miles of sewer main; Over 570 sanitary sewer overflows (SSOs) per year	Estimates for consent decree related improvements estimated at \$400M	Wastewater collection system and treatment plant upgrades (corrective action plans still need to be developed)
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Buy Jackson: The Municipal Special Sales Tax

In 2014, the Citizens of Jackson voted overwhelmingly to support passage of a **1% Sales Tax** for infrastructure improvements. The sales tax applies to goods and services with the *EXCEPTION* of groceries, restaurant food and beverages, prescription drugs, hotel/motel rooms, large equipment purchases and subscription television and internet services. The sales tax is collected by the Department of Revenue, accounted for separately and paid to the City of Jackson. The City maintains the sales tax revenue in a separate special municipal fund in contingency until this master plan, required by the legislation, is approved. The sales tax may only be used to fund capital projects and efforts to pay costs of road and street repair, reconstruction/resurfacing projects and water, sewer and drainage projects.

Infrastructure Master Plan (IMP) and Municipal Special Sales Tax Commission

The legislation for the Municipal Special Sales Tax requires the City to develop an Infrastructure Master Plan (IMP) to capture the proposed projects that will be funded in part or whole by the 1% Sales Tax Revenue. The City has branded the 1% Sales Tax revenue funded infrastructure program as the **Bold New Infrastructure Improvement Program**. The required IMP has been developed as a hybrid master plan and capital improvement plan document that lays out the City's long-term programmatic strategies for infrastructure improvements using this funding source and a capital plan that describes the project implementation strategies over a five year period. This IMP will serve as a roadmap for the Municipal Special Sales Tax expenditures and is required to be voted upon by the Municipal Special Sales Tax Commission also established by the legislation.

This Commission is a 10-member body that shall, **with input from the municipality**, establish a master plan for road and street repairs, reconstruction and resurfacing projects based on traffic patterns, need and usage, and for water, sewer and drainage projects. **Expenditures of the revenue shall be made at the discretion** of the governing authorities **of the municipality** if the expenditures comply with the master plan. The Commission shall **monitor the compliance** of the municipality with the master plan.

As the projects are updated and new projects planned during the CIP planning cycle in the spring of each year, the Commission will vote annually, in the fall of each year, for implementation in the following Fiscal Year.

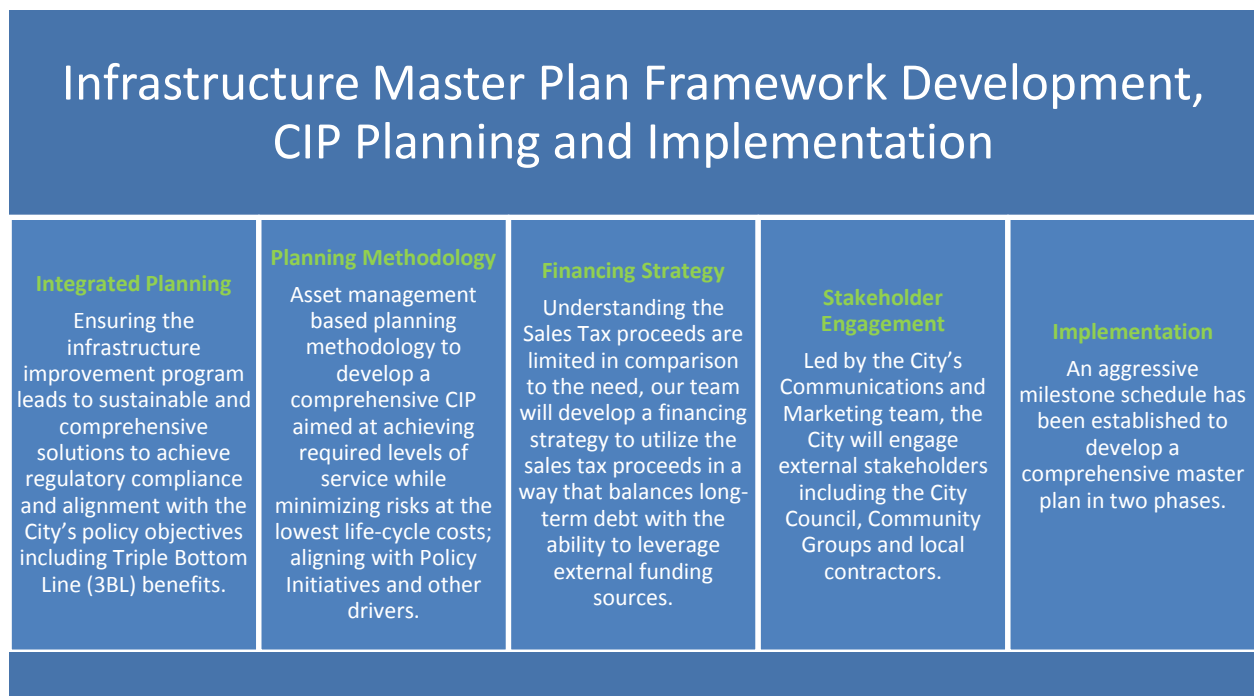
Infrastructure Master Plan Development and Implementation

With limited funding projected from the sales tax revenues (\$13M annually for 20 years), the City understands the need for a sustainable infrastructure investment strategy and one that allows us to balance

improvements in the structural condition of our infrastructure assets while complying with federal mandates for water quality improvements under the Clean Water Act. Such a balance requires an integrated approach to develop and implement holistic solutions that address all of our infrastructure concerns from failing pipes and flooded streets to ADA compliance and advanced traffic controls.

Recognizing the need to balance priorities and leverage limited funding, including the use of innovative financing strategies like public-private partnerships, the City has set out to develop an Integrated Plan using the Integrated Planning Framework Approach released by the Environmental Protection Agency in 2012. The guiding principles and elements of the approach match our philosophy and the direction in which our Department of Public Works is moving to become a best in class service organization.

Through the use of innovative practices like green infrastructure, trenchless technologies, a **DIG ONCE** philosophy and other implementation strategies, our team has developed a framework for sustainable infrastructure investments and achievement of triple bottom line benefits in the City of Jackson. The following graphic summarizes the framework used to develop the Infrastructure Master Plan.



Sustainability and the Triple Bottom Line: Measures of Success

The State of the City's Infrastructure would seem to suggest not only a lack of funding and deferred maintenance, but also the absence of a comprehensive and holistic plan. The Yarber Administration has made **SUSTAINABILITY** the cornerstone of this Infrastructure Improvement Program. Sustainability is:

- Meeting current environmental, social, and economic needs of our community without compromising the ability of future generations to meet these needs

- Improving the quality of life today and laying the foundation to pass on a City that is better than we found it
- A nexus of social equity, economic health and environmental stewardship
- A Foundation for improving quality of life
- **Achieving the “Triple Bottom Line”**

Triple Bottom Line (3BL) is an accounting framework representing the three pillars of sustainability: Social, Economic and Environmental. It is the City’s intent to use this framework as a way to measure the effectiveness of this IMP in ways that matter most to Stakeholders. Establishing 3BL outcomes for this program suggests that making investments in sustainable infrastructure improvements not only repairs physical infrastructure assets, but also achieves these benefits:



- **Social: Quality of Life Improvements**
 - Fair and complete infrastructure restoration for all Citizens in all neighborhoods and communities (ADA requirements)
 - Workforce development program
 - Ensuring "Jobs for Jacksonians" and "Fresh Start" programs are integrated components of IMP projects
 - Exceeding national achievements for minority and women owned businesses participation in both professional services and construction
- **Economic: Creating a Stable Economic Base**
 - Facilitating economic development opportunities by improving the state and capacity of our infrastructure systems to attract new and sustain/grow existing businesses
 - Creating direct and indirect opportunities for job creation
 - Creation of wealth opportunities
- **Environmental: Resilience, Vibrancy and Compliance**
 - Improving the resilience of our communities by investing in infrastructure improvements
 - Improving Jackson’s ability to adapt to climate change impacts
 - Greening the Capital City to make our communities more vibrant, protect natural resources and improve water quality

Capital Plan

Ultimately, the Capital Improvement Plan (CIP) for the Bold New Infrastructure Improvement Program will represent a significant shift in the manner by which capital projects are recorded, reviewed, and eventually executed by the City of Jackson. Like any planning program, it is fully expected the CIP will continue to evolve, adopting changes that will only improve the process and the manner in which the City allocates scarce resources to combat its aging infrastructure. The goal of this City-wide CIP is to identify and prioritize critical infrastructure needs across the City, and to allocate funding to implement those projects through *a process that is logical, transparent and data-driven*.

As part of an initial “gap analysis” the project team reviewed 34 master plan type documents dating back to the mid-1960’s in order to determine the needs of the city and to identify what projects had been

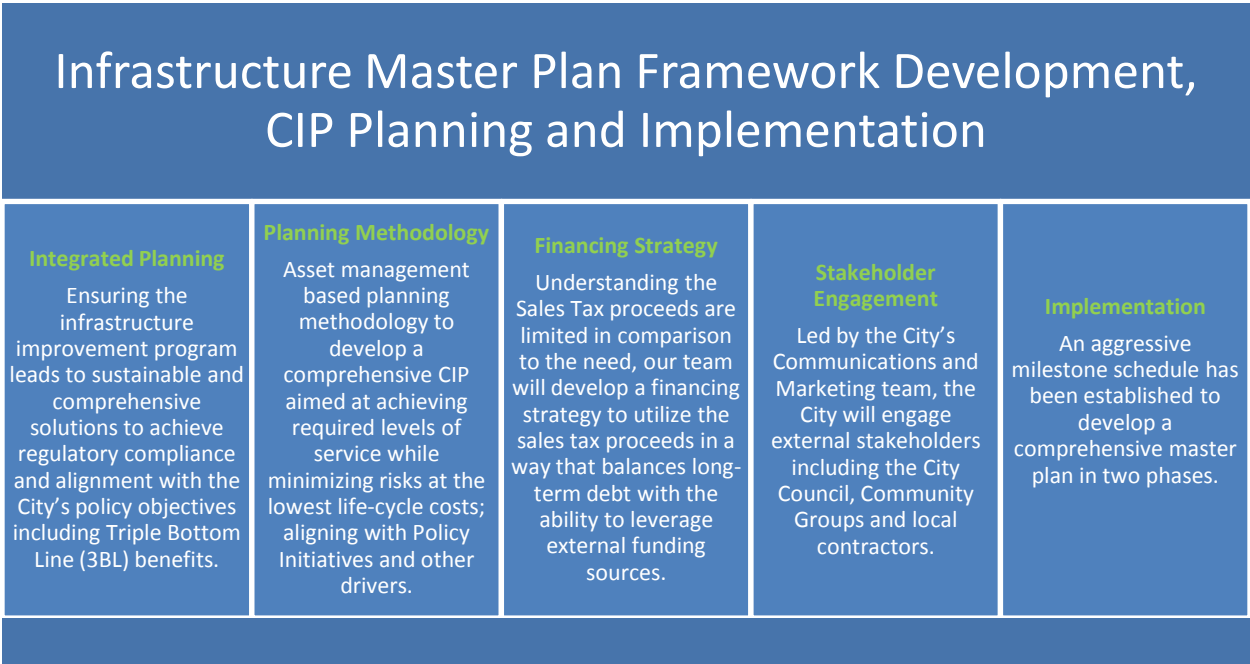
PUBLIC COMMENT DRAFT

planned, completed or deferred. Several data sets have been reviewed including SSO data, drainage issues, service requests and maintenance records to begin planning a five-year capital improvement program. At this time, the CIP for Year 1 has been developed.

Master Plan

A **Master Plan** is generally a comprehensive long range strategy inclusive of analysis, recommendations, and proposals to guide a community’s growth and development based on population, economy, housing, transportation, community facilities, and land use or in this case; *infrastructure improvements and compliance measures*. The legislation for the Municipal Special Sales Tax requires the City to develop an Infrastructure Master Plan (IMP) before the revenues from the 1% Sales Tax can be used to fund infrastructure improvements. In response to this requirement, the City of Jackson has developed this IMP to serve as both a policy based document to guide infrastructure investments and a roadmap in the form of a capital improvement plan to describe specific scope, prioritization and timing of projects in the City of Jackson that will be funded in part or whole by the 1% Sales Tax Revenue.

Infrastructure Master Plan Development

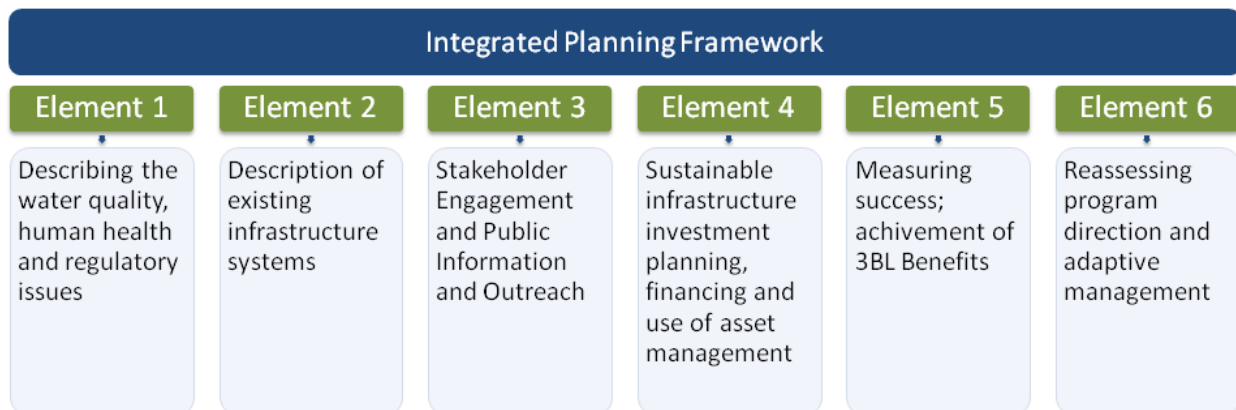


What’s special about the City’s Infrastructure Master Plan is the manner in which it is being developed. Using the ***Integrated Planning Framework Approach*** established by the Environmental Protection Agency (EPA) and an asset management based planning methodology, the City intends to use this master plan as a means to address infrastructure improvement needs in a sustainable holistic way, identify and assess all related infrastructure assets, develop long-term maintenance strategies to preserve infrastructure investments, and capture the opportunity that exists to align Federal Clean Water Act and Safe Drinking Water Act compliance programs while achieving Triple Bottom Line Benefits established by the Yarber Administration.

Integrated Planning

In 2012 the EPA released the *Integrated Municipal Stormwater and Wastewater Planning Approach Framework* to serve as guidance for municipalities like the City of Jackson that have both stormwater (MS4 Program) and wastewater (Consent Decree) compliance programs to manage and implement. **Use of an integrated planning approach assists in identifying critical human health and water quality improvement objectives of the federal Clean Water Act regulations, while balancing the same with infrastructure investments.** It addresses issues with affordability and lends to sustainable infrastructure management. Use of innovative approaches, including green infrastructure practices (bioswales, rain gardens, green roofs, pervious asphalt), to support economic development and enhance the quality of life while meeting compliance requirements. Drivers for integrated planning include:

- Implementation of sustainable and comprehensive solutions that protect public health, enhance quality of life and balances the need to meet regulatory requirements with the need to improve the condition of Jackson’s infrastructure
- Allows the City to meet multiple regulatory requirements in an economical and more affordable manner
- Facilitates achievement of 3BL benefits
- Highlights the use of innovative approaches like green infrastructure and energy efficient technologies
- Promotes concepts like “complete streets” and “green streets”
- Transforms neighborhoods
- Incorporates measures for economic development



Jackson's Triple Bottom Line

Benefits	Programmatic Initiatives	Measures of Impact
Social: Quality of Life Improvements	<ul style="list-style-type: none"> Fair and complete infrastructure restoration for all Citizens in all neighborhoods and communities (ADA requirements) 	<ul style="list-style-type: none"> Number of Projects incorporating ADA Compliant facilities and improvements
	<ul style="list-style-type: none"> Workforce development program 	<ul style="list-style-type: none"> Program training hours Job retention
	<ul style="list-style-type: none"> Ensuring "Jobs for Jacksonians" and "Fresh Start" programs are integrated components of IMP projects 	<ul style="list-style-type: none"> Employment rate in Jackson Reduction in crime and repeat offenders
	<ul style="list-style-type: none"> Exceeding national achievements for minority and women owned businesses participation in both professional services and construction 	<ul style="list-style-type: none"> Increased utilization of minority and female owned businesses
Economic: Creating a Stable Economic Base	<ul style="list-style-type: none"> Facilitating economic development opportunities by improving the state and capacity of our infrastructure systems to attract new and sustain/grow existing businesses 	<ul style="list-style-type: none"> Number of economic development opportunities Growth of Jackson economy Number of new businesses Total number of businesses
	<ul style="list-style-type: none"> Creating direct and indirect opportunities for job creation 	<ul style="list-style-type: none"> Number of new jobs created
	<ul style="list-style-type: none"> Creation of wealth opportunities 	<ul style="list-style-type: none"> Number of new start small businesses
Environmental: Resilience, Vibrancy and Compliance	<ul style="list-style-type: none"> Improving the resilience of our communities by investing in infrastructure improvements 	<ul style="list-style-type: none"> Reduction of infrastructure related complaints Housing investments Reduction in vacant homes
	<ul style="list-style-type: none"> Improving Jackson's ability to adapt to climate change impacts 	<ul style="list-style-type: none"> Reduction of flooding and drainage complaints Increased tree canopy
	<ul style="list-style-type: none"> Greening the Capital City to make our communities more vibrant, protect natural resources and improve water quality 	<ul style="list-style-type: none"> Number of Green Infrastructure practices implemented Increases in tree canopy Water quality improvements Reduction in SSOs

Green Infrastructure



Green infrastructure uses vegetation, soils, and natural processes to manage water and create healthier urban environments. The main components of this approach include stormwater management, climate adaptation, less heat stress, more biodiversity, food production, better air quality, sustainable energy production, clean water and healthy soils, as well as the more anthropocentric functions such as increased quality of life through recreation and providing shade and shelter in and around towns and cities. Green infrastructure also serves to provide an ecological framework for social, economic and environmental health of the surroundings.

The goal to utilize green infrastructure to address regulatory requirements and make communities more vibrant will invite opportunities for collaboration from restoring Smith Park to increasing the City's tree canopy.

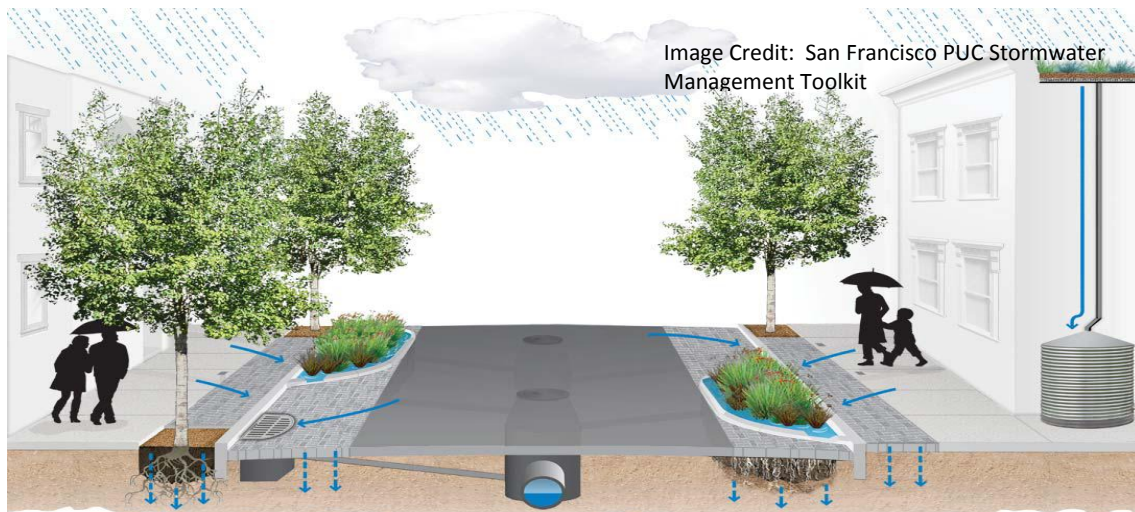
How does Green Infrastructure work?

- **SINK IT (Retention):** Structures that hold stormwater and infiltrate it into the soils; like rain gardens and bioretention areas or green roofs
- **SLOW IT (Detention):** Structures that hold stormwater flow, slowly releasing it to the storm sewer system after the storm; eliminates needs for costly pipe capacity upgrades
- **REUSE IT (Storage and Reuse):** Practices that hold stormwater and uses it to meet non-potable water demands (toilet flushing, irrigation, etc); rain barrels
- **MOVE IT (Conveyance):** Directs flow to a downstream area for storage like drainage swales

Infrastructure Investment Target Areas – Wards, Planning and Economic Development

This Section to be developed with Years 2-5 CIP

Districts – West Jackson, Northeast Jackson, South Jackson, Downtown and Midtown



Council Wards – Infrastructure Improvement Areas of Focus

Ward 1

- **Ridgewood Road**
 - Location: Bordered from Countyline Road to Northside Drive
 - Challenge: Significant road deterioration and deformation
 - Proposed action: Resurfacing all lanes
- **State Street**
 - Location: Bordered from Meadowbrook Road to Cedars of Lebanon
 - Challenge: Significant road deterioration and deformation
 - Proposed action: Resurfacing all lanes
- **Old Canton Road (including Canton Mart)**
 - Location: Bordered from I-55 Frontage Road to Adkins Boulevard
 - Challenge: Significant road deterioration and deformation
 - Proposed action: Resurfacing all lanes
- **White Oak Creek**
 - Point of access: Briarwood Road

- Challenge: erosion; ROW issues involving multiple properties; and overgrowth
- Proposed action: implement approximately one (1) mile of improvements

Ward 2

- **Country Club Drive Bridge**
 - Challenge: Timber bridge that needs to be replaced to accommodate increased traffic loading
 - Proposed action: Expansion of the bridge and roadway
- **Presidential Hills**
 - Location: Entire subdivision
 - Challenge: Major flooding due to drainage issues
 - Proposed action: Resolve drainage issues
- **Hanging Moss Creek**
 - Location: Tributary 5-1 and 4-1
 - Challenge: Erosion, overgrowth on the banks and private ROW
 - Proposed action: Creek stabilization

Ward 3

- **Medgar Evers Boulevard**
 - Location: Five Point (Intersection of Medgar Evers, Woodrow Wilson, Livingston Rd and Gordon Street) to Northside Drive
 - Challenge: Significant road deterioration and deformation; drainage issues; water and sewer line failures; and multiple utility cuts
 - Proposed action: Comprehensive water and sewer line repairs; resurfacing all lanes; enhancement of drainage capacity
- **Mayes Street & Bridge**
 - Challenge: Sufficiency rating of nine (9) out of 100; Significant road deterioration to concrete
 - Proposed action: Replacement of bridge

Ward 4

- **Brookhollow Circle**
 - Location: Entire neighborhood
 - Challenge: Major drainage hindrances
- **Officer Thomas Catchings Drive**
 - Challenge: Sewer line failure leading to sewage flow into Lynch Creek
- **Clinton Boulevard at Sylvan Trail**
 - Location: in vicinity of Lakewood Funeral Home
 - Challenge: Existing asphalt deteriorates consistently at this location due to the traffic load and soil condition creating a safety issue

Ward 5

- **Robinson Road**
 - Bordered from Ellis Avenue to Capitol Street

- Significant road deterioration and deformation
- **Capitol Street**
 - Boling Street to Prentiss Street
 - Significant road deterioration and deformation
- **Lynch Creek**
 - Erosion, overgrowth on the banks and private ROW

Ward 6

- **Terry Road**
- **Caney Creek**
- **McDowell Road**

Ward 7

- **Charles Tillman Bridge**
 - Location: near Whitfield Street and Mill Street
 - Challenge: Deferred maintenance and original design constraints
- **State Street**
 - Location: Fondren Road to Meadowbrook Road
 - Challenge: Significant road deterioration and deformation; water and sewer line failures
- **Choctaw Road**
 - Location: Eubanks Creek
 - Challenge: Major flooding with heavy rain

Planning and Economic Development

- Comprehensive Plan
- Major Gateways
- Parks and Green Space
- Economic Development Opportunities

Planning Methodology

Asset Management has proven to be a sustainable best practice that improves a Municipality's ability to meet the demands of providing service and making sound decisions to improve the state

Asset Management is an integrated, multi-disciplinary set of strategies/best practices focused on sustaining/extending the service life of public infrastructure assets at the lowest life-cycle cost. Given the \$750M to \$1B the City will need to make to improve the City's infrastructure: fix drainage issues, repair and replace bridges, rebuild roads, replace water and sewer pipes; an Asset Management program is being launched to adequately protect those investments and make on-time decisions to repair, replace or upgrade infrastructure in the future. Part of the 1%

sales tax funding will be used to fund the asset management program by purchasing new capital equipment for routine and preventive maintenance; funding for activities related to identification, condition and criticality assessments of existing infrastructure assets to allow for capital improvement

project planning and risk assessment. The **planning methodology for the IMP capital improvement plan will remain asset management based** as we work to better understand the condition of the systems and assets we manage, plan interim and long-term measures that minimize the risk of failure and impacts to citizens, develop strategies to finance the improvements and maintain the infrastructure long after the investments have been made.



Financing Options

The critical elements to this infrastructure master plan are:

- **The Approach:** Presents a strong package to investors because it speaks to management, the priorities of the City and long-term preservation of investments
- **Capital Plan:** Expresses discrete projects and costs to communicate needed investment and specific funding needs
- **Financing:** Identifies viable options for the City’s team to pursue to get the work funded and solutions in the ground

*How does an estimated \$240M
Municipal Special Sales Tax
fund a \$1Billion need?*

Because the City is targeting upcoming grant funding opportunities and appropriations for surface transportation, surface water and water program funding; this plan has given us the opportunity to align the projects with the selection criteria on which grant funding is based.

Jackson’s project team has reviewed the revenues being generated by the sales tax and developed a list of viable financing options available to the City to leverage those funds.

Public Financing

In the United States, municipal projects are typically financed using some type of public funding. Sources of public funding include debt, internal/reserve funds, and federal or state grants. Debt can either take the form of a bond or a loan. A bond, which is the primary instrument of municipal debt, is a written promise to repay borrowed money on a definite schedule usually at a fixed rate of interest for the life of the bond. The key element of municipal bonds for Jackson is that they are tax-exempt, meaning that investors do not pay taxes on the interest proceeds from the bonds. As a result, municipalities are able to set much lower interest rates on their bonds and still have them be attractive to investors. These lower interest rates represent a significant savings to the issuing government when compared to the interest rates they would be required to offer if their bonds were not tax-exempt.

Short-Term, Tax-Exempt Debt. Short-term tax-exempt debt consists primarily of short-term municipal bonds and short-term securities known as notes. These instruments generally have maturities ranging from a few months to a few years, have fixed interest rates, and are issued in anticipation of a bond issue, grant proceeds, or tax collections. Examples include bond anticipation notes (BANs) which are issued in anticipation of the sale of long-term bonds; grant anticipation notes (GANs), issued in anticipation of receipt of state or federal grants; tax anticipation notes (TANs), issued in anticipation of the receipt of taxes; revenue anticipation notes (RANs), issued in anticipation of revenues to be generated by the issuing utility; and tax and revenue anticipation notes (TRANs).

Short-term debt markets are easily accessed and the interest rates associated with these instruments are typically low. As a result, short-term instruments are commonly used to meet short-term capital needs for design and initial construction while waiting for long-term financing.

General Obligation Bonds. General obligation bonds are secured by the full faith and credit of the issuing entity. Typical issuers of general obligation bonds include municipalities, counties, special service districts with taxing authority, and other government entities. General obligation bonds have a major advantage in that they are backed by the full taxing capacity of the government entity, and consequently this credit is usually regarded as having the strongest security pledge available and therefore, the lowest available net interest cost. General obligation funding is particularly applicable for projects such as streets and roads that are not associated with a direct source of revenue. There are, however, disadvantages to general obligation bonds. Since the issuance of general obligation debt typically requires legislative or voter approval, project funding can be delayed significantly particularly when the project being funded has a high cost or is politically sensitive. In addition, municipalities typically have a limit on the amount of general obligation debt that they can hold at any one time. Therefore, it is important to take long-term capital needs into account when considering the use of general obligation bonds.

Revenue Bonds. Revenue bonds are debt instruments for which interest and principal are payable solely from the revenue generated from a specific project or utility. In most states, the holders of revenue bonds do not have recourse to have taxes levied to pay required debt service. A major advantage of revenue bonds is that they preserve the issuer's general obligation debt capacity for other projects. Revenue bonds are generally tax-exempt and would be issued at interest rates lower than taxable debt but higher than general obligation debt. The current rate on AAA rated, tax exempt revenue bond is approximately 2.7% while the rate on A rated bonds is about 3.5%. Since revenue bonds are secured by revenues associated with the projects they are used to fund, they are not a viable source of funding for projects without a discrete source of revenue such as streets and government buildings.

Green Bonds. Green bonds are a relatively new funding option that can be used for "green" or environmentally friendly projects. While these bonds are typically used to fund climate related improvements such as renewable energy projects, DC Water is funding major drainage system projects with these types of bonds. Green bonds are not tax-exempt, but the interest rates on these bonds are typically lower than other taxable bonds because altruistic investors forgo larger returns in support of environmentally friendly projects. One major drawback of these bonds is the need for the project to be certified as green which is a lengthy and complicated process.

State Revolving Funds. The 1996 Safe Drinking Water Act Amendments authorized the funding of Drinking Water State Revolving Loan Funds (SRFs) to assist drinking water systems in financing the infrastructure costs of complying with the Act and to protect the public health. SRFs provide low-cost loans to publicly and privately owned water and wastewater systems, as well as nonprofit community ones, for periods up to 20 years. Many states, including Mississippi, leverage funds provided from the federal and state government by issuing SRF revenue bonds. As this program is changing it is anticipated that limited funding will be available in the future and while SRF's may be a source of funding for Jackson's water and sewer utilities, these funds are not available for other types of projects.

Federal/State Grants. Federal and state grants are another potential funding source for utility capital projects. A grant is a sum of money awarded to an eligible entity without a demand for repayment. Typically, grants are awarded by the federal government to State or local governments, or by States to local governments, for the purpose of financing a particular activity or facility.

Since there are no requirements for the repayment of grants, these are very low cost financing tools. However, there is intense competition for a very limited supply of grant funding and typically these grants are reserved for municipalities that are unable to utilize other financing tools. In addition, grant funds often have conditions that affect the scope, intent, nature or cost of the project or program in question, therefore, by accessing these funds, a utility may have to forfeit a significant amount of control over the project.

Government Loans. There are a number of government agencies that offer loans to municipalities to fund infrastructure improvements. New programs and additional funding has been allocated to funds loan programs like WIFIA the Water infrastructure Finance and Innovation Authority; and TIFIA – Transportation Infrastructure Finance and Innovation Authority. WIFIA and TIFIA access funds from the U.S. Treasury at long-term Treasury rates and use those funds to provide loans or other credit support for water and transportation related projects.

Private Financing

When a municipal agency is unable to utilize low cost public financing, they may be able to turn to private sources of financing. Private financing is typically more expensive because private companies usually expect a return on their investment that is usually several hundred basis points higher than the cost of tax-exempt debt. Two relatively common private financing options are described below

Direct Source. Direct source is a financing mechanism that involves a partnership with a private company whereby the supplier of a piece of equipment provides financing. The utility then makes regularly scheduled payments to the supplier. This financing tool is generally not appropriate for large scale projects and is typically only used to finance individual components of a project.

Public Private Partnerships. In the US, the growing public sector financial constraints and aging infrastructure systems has presented a need to think beyond traditional financing models and be receptive to partnerships with the private sector to get projects funded. Other agencies are turning to P3 project delivery methods and financing models to leverage limited public resources. The City has already proposed projects to the US Department of Transportation for the P3 Pipeline of projects in development

including a Safe Bridges Program of projects and Greening the Capital program focused on greening the City of Jackson's gateways.

Stakeholder Engagement

- **Seven Wards in Seven Days**
 - Mayor/Council Members to tour critical infrastructure issues in each Ward followed by a Listening Tour Stop focused on infrastructure deficiency impacts in the communities
- **Local Contractors Roundtable**
 - To brainstorm ideas on local economic enhancement measures; hear what works for capacity building
- **Public Comment Period**
 - Draft plan will be posted for public comments which will then be considered for incorporation into the plan

Implementation Strategy

Program Management

- Program management is used to deliver “outcomes”(3BLs, reducing citywide flooding, etc) through on-time, on-budget project delivery; coupled with a new internal program manager designated for the IMP Program, the City is proposing to utilize the services of a program management team to assist the City by augmenting internal staff resources and provide engineering and inspection expertise to perform as-needed project planning, design and construction inspection services
- Important program needs to stay on track; City staff needs to focus on operations, implementation of the new programs, and project delivery for the comprehensive CIP program
- Budgeted estimate included in Year 1 CIP for approval of use of 1% Sales Tax revenues to fund the PM; on-going costs anticipated
- Program manager will be responsible for CIP Development, Green Infrastructure Strategy Development, GI Challenge Launch and facilitation; as-needed project design and inspection services (so critical issues that arise do not have to wait for CIP programming) and IMP CIP project delivery

Stakeholder Engagement/Public Information and Outreach

- Continue will Public Comment period to finalize draft and incorporate comments into years 2-5.
- External stakeholder engagement
- Regular PIO under PM assignment

Routine and Preventive Maintenance Programs

- Protecting capital investments

- Part of funding recommended to support in-house programs to maintain infrastructure after investments are made
- Equip operation to have ability to address issues like chronic flooding as capital projects are being programmed

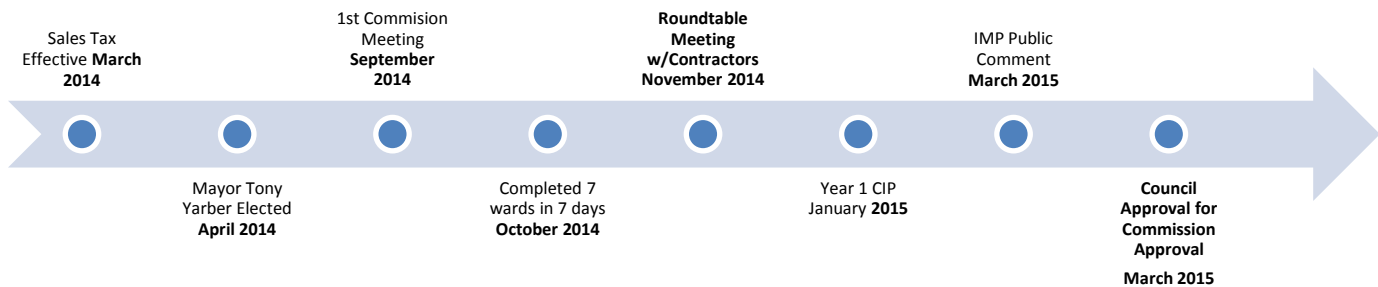
Capital Improvement Program

- First CIP since 2003
- Programmed projects with a 5-Year planning horizon
- Provides for the ability to attract and demonstrate need for funding

Green Infrastructure Challenge

- Review suitability of GI in Jackson
- Implement projects in the first year

Timeline



Jackson's Infrastructure Improvement Needs

Existing Infrastructure Master Plans Gap Analysis

Over the years the City has commissioned several master plans or facilities plans for infrastructure improvements; however, many of the project recommendations have not been implemented due to funding constraints and/or changes in priority, operating conditions and regulatory need. Therefore, one of the first steps to identify the needed investments and start development of the five-year capital improvement plan involved the Project Team collecting and reviewing 34 existing plans and other pertinent documents dating back to the mid-1960's in order to perform a "gap analysis". The Team reviewed what projects had been completed, what in-house work had been completed, planned projects and known issues that had not yet been accounted for in project planning. This process assisted the City to quickly determine the types of projects that need to be planned and most importantly what projects are "shovel-ready". This process also helped to assess whether or not we have the information needed to plan a long-term capital plan or if additional studies are needed. For instance, the most recent drainage and water system master plans were limited in scope. In order to plan improvements for the life of the 1% Sales Tax program (20 years); the City will need to perform more comprehensive assessments to identify the source of chronic issues and develop sustainable solutions. Therefore, the capital plan for the 1% Sales Tax Program will incorporate studies that will identify required improvement projects in addition to design and construction projects. This process also highlighted a need to have more information on the

location, function and condition of our assets to properly scope project work for the future and adequately maintain the systems.

Drainage Improvements

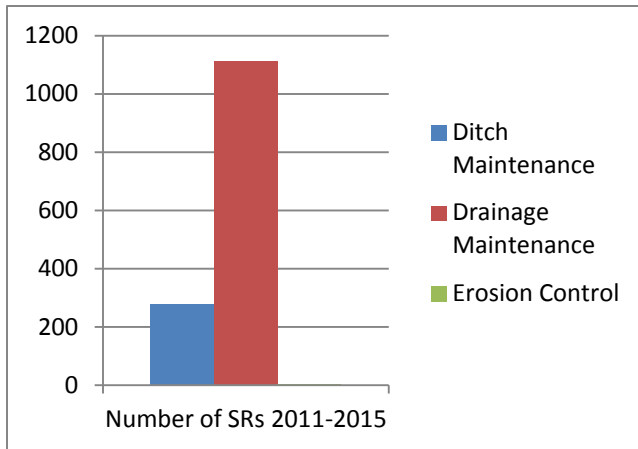
The City of Jackson is responsible for operation and maintenance of a separate stormwater drainage system referred to as the Municipal Separate Storm Sewer System (MS4). Stormwater drainage in the City of Jackson is typically collected in roadside ditches, open channels, catch basins and storm drains then conveyed to major drainage channels and ultimately the Pearl River. The City of Jackson’s Stormwater Drainage System is comprised of 15 major drainage channels, over 200 minor tributaries, small streams and roadside drainage ditches. The miles of separate storm drains, number of storm drain manholes, storm catch basins/storm drain inlets must be determined.

City of Jackson's Major Drainage Channels			
White Oak Creek	Belhaven Creek	Lynch Creek	Trahan Creek
Purple Creek	Bogue Chitto Creek	Three Mile Creek	Big Creek
Eastover Creek	Eubanks Creek	Hardy Creek	Smith Creek
Hanging Moss Creek	Town Creek	Cany Creek	

State of existing assets

The major drainage channels, minor tributaries and streams that flow through the City’s neighborhoods, in many areas adjacent to private property, have degraded over time with heavy rainstorms causing scour and erosion. The erosion of these natural channels and drainage ways has led to loss of property and in some cases threats to public health and safety. During heavy or at times moderate rains, the City’s storm drain system becomes overwhelmed leading to flooded streets and standing water. This issue is attributed to the capacity of the system to handle the amount of rainfall, the condition of the system that inhibiting the proper continuous flow of drainage and difficulty in proactively maintaining drainage ways and the storm drain infrastructure to keep them clear of debris.





While the Bridges and Drainage Section of the Infrastructure Management Division is responsible for maintaining the drainage system and carries out activities including repairing and replacing catch basins, storm drain inlets, storm drain pipes, and removing debris from drainage channels to promote proper drainage flow, limited resources has made it difficult to keep up with service level demands.

This section is responsible for implementing a Pipe Replacement Program. Many of our storm drain pipes are made of corrugated metal which have begun to rust and deteriorate, and the purpose of this program is to replace those storm drain pipes to promote proper drainage. This section is also responsible for trimming and removing dead trees from the City's right of ways, and also removing trees that have fallen into drainage channels. During adverse weather, Bridges and Drainage is also responsible for removing fallen trees from City streets and responding to areas that are experiencing flooding issues.

Compliance Programs/Priorities

As part of the Federal Clean Water Act, the U.S. Environmental Protection Agency (US EPA) has developed stormwater management regulations as part of a National Pollutant Discharge Elimination System (NPDES) permitting program to improve the water quality of surface waters like the Pearl River. Under this program, the City of Jackson is considered a Phase I MS4 NPDES permitted municipality and required to hold a 5-year permit for management of stormwater in the City. The City is on its second generation of the permit and will be required to renew its MS4 Phase I Permit in 2017.



As part of the requirements for the program, the City is required to develop and maintain a stormwater management program that implements best practices to protect surface water quality and prevent high volumes of stormwater runoff, or drainage, from causing flooding. Best practices include structural improvements to address erosion, increase capacity of the storm drainage system and implementation of green infrastructure practices that retain and filter stormwater drainage; or non-structural practices like implementation of routine and preventive maintenance programs. The City is required to track the progress towards these

goals, implementation of best practices and the requirements in the MS4 Permit to provide a report to MDEQ on an annual basis. The MS4 program also includes six control measures for which the City is required to implement best practices:

- Public Education and Outreach

- Public Involvement and Participation
- Illicit Discharge Detection & Elimination
- Construction Site Stormwater Runoff Control
- Post-Construction Stormwater Management in New Development and Redevelopment
- Pollution Prevention and Good Housekeeping for Municipal Operations

Programmatic Initiatives

It is anticipated that the MS4 permit renewal in 2017 will bring with it requirements to use innovative practices including green infrastructure and low impact development to better manage stormwater drainage. This program provides an opportunity to restructure the City's stormwater management program, update best practices and implement a green infrastructure strategy in the City that meets several goals and objectives including increasing the City's tree canopy. This program also provides an opportunity to implement green solutions along drainage ways that not only stabilize channels but also provide water quality benefits.

In order to develop comprehensive sustainable solutions to localized drainage issues, the City understands that it must develop a more comprehensive plan, watershed by watershed, to identify opportunities for improvement, solutions to improve the drainage system and protect existing flood prone areas. This will require comprehensive hydraulic and hydrologic modeling as well as identification, characterization and mapping of existing drainage system components.

Based on historical data and the high volume of drainage complaints during and after storm events, DPW recognizes a need to implement a routine and preventive maintenance program that limits the impacts of high frequency rainstorm events. This program will be implemented by in-house staff as part of the Department's Asset Management Initiative.

Historical Investments and Funding Sources

The City has typically funded drainage improvements with General Funds, Community Development Block Grant Funds, and Federal Funds allocated by the Natural Resources Conservation Service to provide for open channel stabilization and widening projects. In house crews have implemented a pipe replacement program to address chronic drainage issues and supplement limited capital resources. Completed projects are listed in the table below.

PIPE PROJECT LIST						
LOCATION(S)			PIPE SIZE		LENGTH	
			Inches	Feet		REMARK(S)
1	1127	Dardenelle	18	140	HDPE	
2	119	Dendron Drive	30	60	HDPE	COMPLETED
3	119	Yucca Drive	36	60	HDPE	COMPLETED

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4	123	Cedarwood Drive	18	160	HDPE	COMPLETED
5	1320	Wooddell Drive	30	160	HDPE	
6	140	Chippeewa Circle	18	100	HDPE	COMPLETED
7	1550	Burton Street	30	120	HDPE	
8	1624	Westbrook Road	30	160	HDPE	
9	190	Rivera Drive	18	60	HDPE	COMPLETED
10	221	Idlebrook	42	200	HDPE	COMPLETED
11	230	Rivera Drive	18	160	HDPE	COMPLETED
12	2403	Wild Valley Road	36	300	HDPE	
13	270	Mikell Street			HDPE	
14	2870	Rosehaven Drive	30	220	HDPE	
15	3070	Suncrest Drive	40	300	HDPE	COMPLETED
16	3257	Simwood Drive	24	200	HDPE	COMPLETED
17	353	William McKinley Circle	24	80	HDPE	COMPLETED
18	368	DeWitt Avenue	30	140	HDPE	COMPLETED
19	4248	Oak Hill Drive`	30	200	HDPE	
20	4870	Gaylyn Drive	24	300	HDPE	
21	4970	Brookwoode Drive	65 X 40	100	ACP	COMPLETED
22	5319	Pine Lane drive	24	160	HDPE	
23	5332	Queen Eleanor Lane	24	240	HDPE	COMPLETED
24	5414	Queen Mary Lane	30	260	HDPE	
25	5911 & 5917	Whitestone Road	18	140	HDPE	
26	6035	Woodhaven Road	24	160	HDPE	
27	6058	Huntview Drive	30	180	HDPE	
28	6201	Winthrop Circle	30	200	HDPE	
29	6219	Winthrop Circle	30	400	HDPE	
30	6247	Tanglewood Drive	24	200	HDPE	COMPLETED
31	6324	Woodstock Drive	24	200	HDPE	
32	720	Nakoma Drive	18	140	HDPE	
33	7272	Woodhill	24	200	HDPE	COMPLETED
34	758	Primose Avenue	18	100	HDPE	COMPLETED
35	832	Beechwood Court	24	140	HDPE	
36	917	Royal Oak Drive	48	120	HDPE	
37		Manchester Street	36	860	HDPE	
38		Manchester Street	24	200	HDPE	

	Central Street		300	HDPE
334	Bell Street		160	HDPE
4748	Village Drive	30	200	HDPE
231	Mitchell Avenue	18	200	HDPE
109	Dendron Drive	18	120	HDPE
39	Manchester Street	44 X 27	60	ACP

Investment Priorities

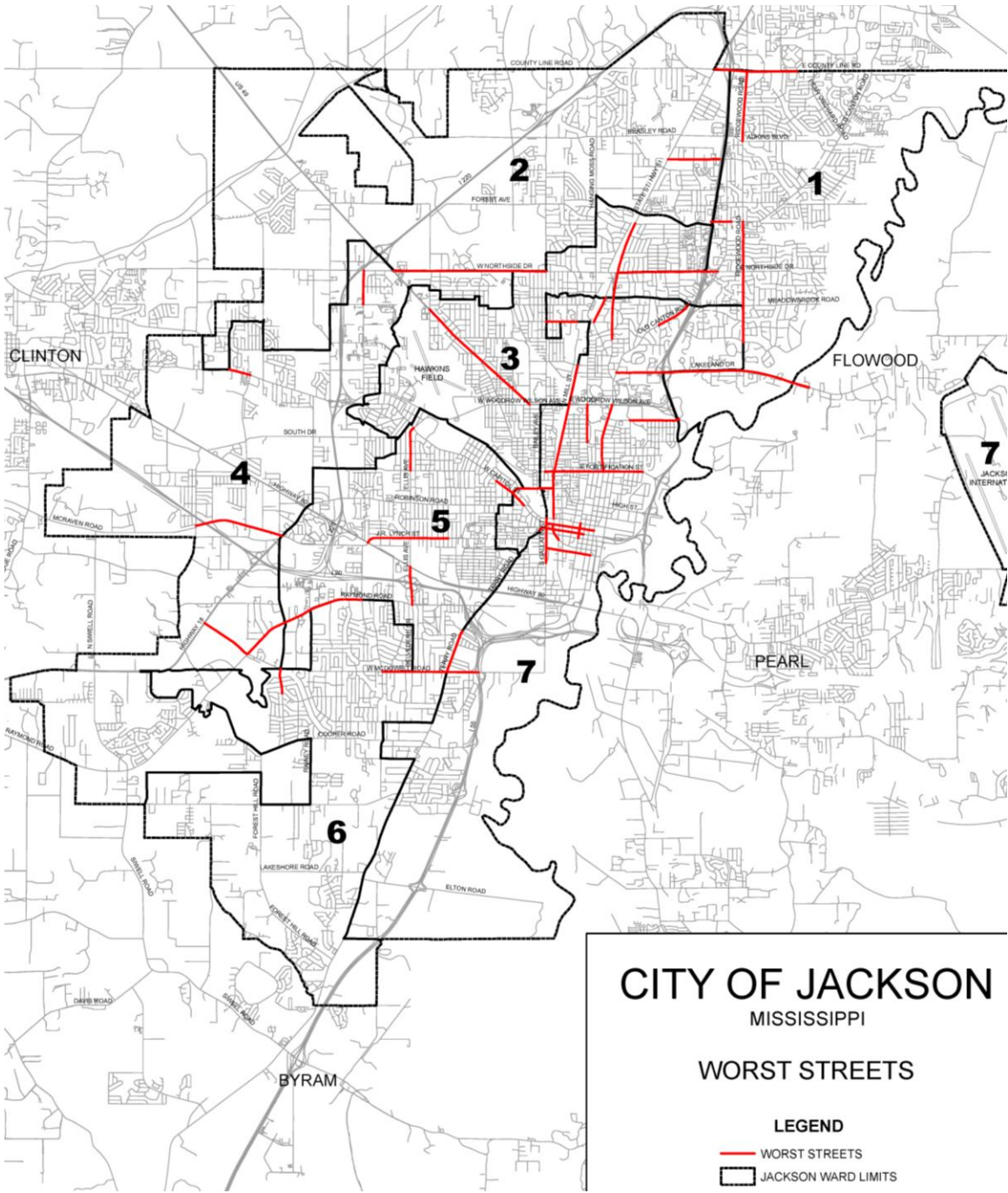
The limited investment in upgrading the City's drainage system and making much needed improvements throughout the City has been due to not having the available funds or a sustainable dedicated funding source. As evidenced by the number of service requests for drainage related issues, the damage to public and private property caused by repeated flooding; the miles of drainage ways including road-side ditches that need to be assessed and addressed; **drainage improvements in the City of Jackson is an investment priority for use of the 1% Sales tax funds.** Specific investment priorities include:

- Neighborhood drainage improvement projects that will mitigate localized flooding in communities.
- Regional stormwater management improvement projects including regional detention systems to control runoff.
- Watershed restoration projects incorporating upland stormwater quality and quantity control practices to improve surface water quality; limit sediment erosion and mitigate flooding by controlling stormwater flows to downstream areas.
- Drainage channel improvement projects that where feasible, restore the channel to a natural state to improve water quality and stabilize the channel to protect against erosion. Project types include stream restoration, channel stabilization/enhancement and flood control. Priority segments of major drainage channels are presented in the following Table.

Roads and Bridges

The City of Jackson is responsible for maintenance of 2055 lane miles of roadway in the City of Jackson. This includes major gateways, arterials, collector roads and residential neighborhood streets. In addition to responsibility to maintain roads the City is also responsible for maintaining associated signage and the signalization network including above and below ground infrastructure including over 2,000 signals across the City and over 300 intersections. Adjacent sidewalks, where they exist, and other structures in the City's right-of-way must also be maintained.





State of existing assets

In addition to drainage related complaints, potholes and utility cuts contribute to the large volume of service requests received by the City. In 2011, the Central Mississippi Planning and Development District performed a pavement condition assessment of all the principal and minor arterial streets and collector streets within Hinds County including an assessment of the pavement condition on a scale from 1 (good condition) to 5 (worst condition). From this assessment, a list of the Top 50 Worst streets was generated in 2013 and those streets are in the City of Jackson.

Top 50 Worst Streets for City of Jackson					
Developed by Central Mississippi Planning and Development District					
Name	Maintenance Category	Condition	Roughness	From Street	To Street
Boling St 2	Rebuild/Replace, Asp	5	3	Northside Dr	Michael Avalon Dr
Briarwood Dr 2	Rebuild/Replace, Asp	5	3	I-55	State St
Canton Mart Rd	Rebuild/Replace, Asp	5	3	Old Canton Rd	I-55
Capitol St 6	Rebuild/Replace, Asp Over brick & conc	5	3	Lamar St	State St
Clinton Blvd 3	Rebuild/Replace, Asp	5	3	Westhaven Blvd	Magnolia Rd
Ellis Ave 1	Rebuild/Replace, Asp	5	3	Capitol St	St. Charles St
Fortification St 1	Rebuild/Replace, Asp	5	3	Greymont Ave	State St
Fortification St 2	Rebuild/Replace, Asp	5	3	State St	West St
Fortification St 4	Rebuild/Replace, Asp	5	3	Lamar St	Wood St
Gallatin St 2	Rebuild/Replace, Asp	5	3	Amite St	South St
Lynch St 2	Rebuild/Replace, Asp	5	3	Wiggins St	Hwy 80
Lynch St 5	Rebuild/Replace, Asp	5	3	Robinson Rd	Wiggins Rd
McDowell Rd A-1	Rebuild/Replace, Asp	5	3	McDowell Rd Ext	Raymond Rd
Medgar Evers Blvd 5	Rebuild/Replace, Asp	5	3	ML King Jr Dr	Woodrow Wilson Ave
Mill St 1	Rebuild/Replace, Asp	5	3	Mitchell Ave	Woodrow Wilson Ave
Mill St 2	Rebuild/Replace, Asp	5	3	Woodrow Wilson Ave	Amite St
Mill St 4	Rebuild/Replace, Asp	5	3	Capitol St	Roach St
N. West St 4	Rebuild/Replace, Asp	5	3	Woodrow Wilson Ave	Adelle St
Northside Dr 12	Rebuild/Replace, Asp	5	3	State St	Manhattan Rd
Northview Dr 2	Rebuild/Replace, Asp	5	3	Meadowbrook Rd	Mayes St
Old Canton Rd 6	Rebuild/Replace, Asp	5	3	Meadowbrook Rd	Cane Creek Blvd

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Pearl St 3	Rebuild/Replace, Asp	5	3	Congress St	Mill St
Ridgewood Rd 4	Rebuild/Replace, Asp	5	3	Adkins Blvd	County Line Rd
Riverside Dr 1	Rebuild/Replace, Asp	5	3	Highland Dr	Peachtree St
S. West St 4	Rebuild/Replace, Asp	5	3	Pascagoula St	Amite St
South St 2	Rebuild/Replace, Asp	5	3	State St	Gallatin St
State St 7	Rebuild/Replace, Asp	5	3	Seneca Ave	Meadowbrook Rd
State St 8	Rebuild/Replace, Asp	5	3	Meadowbrook Rd	Northside Dr
State St 9	Rebuild/Replace, Asp	5	3	Northside Dr	Sheppard Rd
Suncrest Dr 1	Rebuild/Replace, Asp	5	3	McDowell Rd	Smallwood St
McDowell Rd 2	Rebuild/Replace, Asp	5	2	McFadden Rd	I-55
Medgar Evers Blvd 4	Rebuild/Replace, Asp	5	2	Sunset Dr	ML King Jr Dr
Northside Dr 13	Rebuild/Replace, Asp	5	2	Manhattan Rd	Old Canton Rd
Capitol St 4	Overlay, Asphalt	4	3	Magnolia St	Robinson Rd
Gallatin St 3	Overlay, Asphalt	4	3	South St	Hiawatha St
Mayes St 2	Overlay, Asphalt	4	3	Bailey Ave	West St
Monument St 2	Overlay, Asphalt	4	3	Mill St	Capitol St
Raymond Rd 4	Overlay, Asphalt	4	3	Castle Hill Dr	McDowell Rd
Raymond Rd 5	Overlay, Asphalt	4	3	McDowell Rd	McDowell Rd Ext
Terry Rd 3	Overlay, Asphalt	4	3	Angle St	McDowell Rd
Terry Rd 5	Overlay, Asphalt	4	3	Pavement Change	Stokes-Robertson Rd
Lakeland Dr 1	Overlay, Asphalt	4	2	Old Canton Rd	St. Dominics Hospi
Northside Dr 10	Surface Coat, Asphal	3	2	California Ave	Bailey Ave
Northside Dr 9	Surface Coat, Asphal	3	2	Medgar Evers Blvd	California Ave
State St 4	Surface Coat, Asphal	3	2	Fortification St	Woodrow Wilson Ave
County Line Rd A-2	Surface Coat, Asphal	3	1	Northpark Mall	Ridgewood Rd
County Line Rd A-3	Surface Coat, Asphal	3	1	Ridgewood Rd	Hwy 51
Ellis Ave 3	Crack Seal, Asphalt	2	2	Hwy 80	Raymond Rd

PUBLIC COMMENT DRAFT

Ridgewood Rd 2	Crack Seal, Asphalt	2	2	Eastover Dr	Old Canton Rd
Lakeland Dr 2	Crack Seal, Asphalt	2	1	St. Dominics Hospi	Ridgewood Rd
Lakeland Dr 3	Patch, Asphalt	2	1	Ridgewood Rd	Pearl River

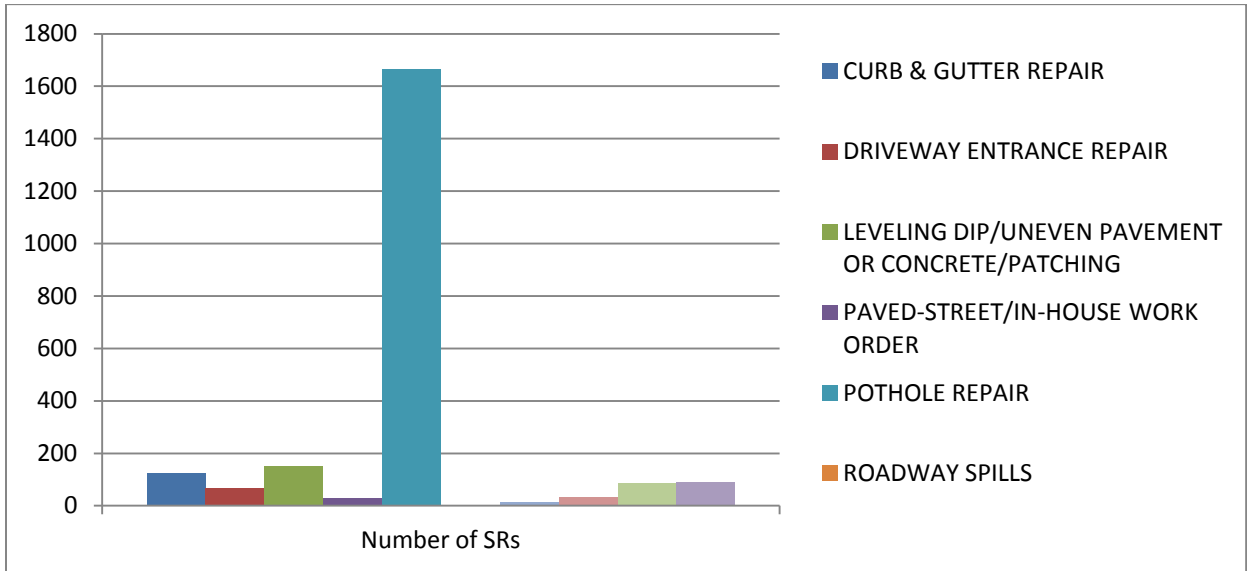
The assessment also showed that 30% or more of the streets in the City require some level of reconstruction rather than resurfacing. Reconstruction is more costly at an estimated \$400,000 per lane mile versus \$60,000 per lane mile of street resurfacing. Of the Jackson Streets that are rated in the Hinds County Pavement Management System, over 66% of those streets have a condition rating of 5.

The Traffic Maintenance Section of the Infrastructure Management Division maintains 324 traffic signals within the city limits. This number includes 47 signals on Mississippi Department of Transportation highways that the City must maintain by statute. Approximately half of the traffic signals are operated by equipment cabinets that are 25 to 40 years old and/or controllers that are over 15 years old. Most signals utilized in-ground loop detectors that are subject to frequent cuts and failures due to poor asphalt and shifting yazoo clay. Traffic maintenance also provides limited maintenance to the traffic monitoring system that is shared with MDOT.

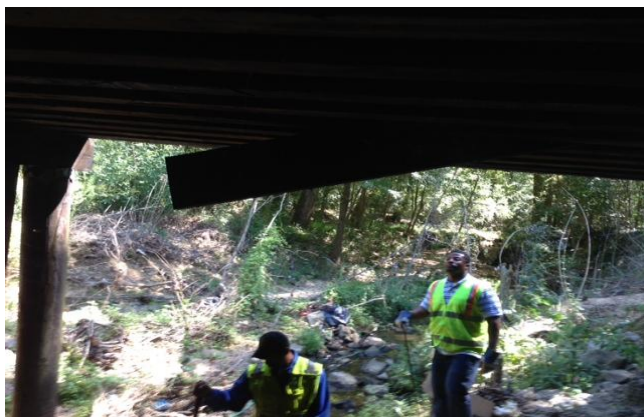
The traffic signal system is operated by ACTRA, a master signal operating system that is now outdated and in need of total replacement. The fiber optic cable and copper interconnect cable from the central server at the Eudora Welty Library to many of the signals have a number of maintenance issues that results in some signals not communicating with the master server resulting in timing patterns and plans that must be manually adjusted. The result of a traffic signal operational system with so many small issues is traffic delays, lack of peak hour flow and progression, and longer queues at intersections.

The Traffic Maintenance Section has for at least 10 years been operating on a small budget and older equipment that limits their repair capabilities. Their current capabilities are emergency signal repairs after an accident, small repairs such as signal head and related signage replacement, and major detection failures as noted by maintenance or engineering staff. The budget has not supported larger repairs such as replacement of knocked down mast arm or strain poles, routine cabinet and controller replacement, detection system upgrades. Frequent vehicle failures and repairs further limit repair abilities and is a sign that some equipment needs to be budgeted for replacement.

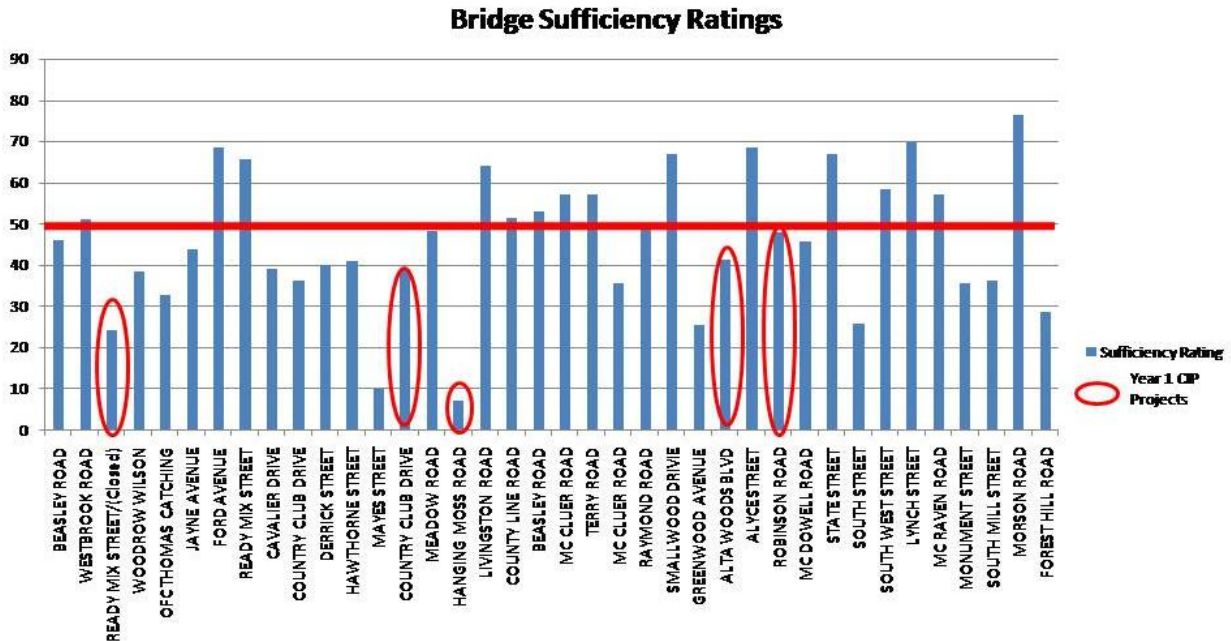
While the Bridges and Drainage Section of the Infrastructure Management Division is responsible for maintaining the drainage system and carries out activities including repairing and replacing catch basins, storm drain inlets, storm drain pipes, and removing debris from drainage channels to promote proper drainage flow, limited resources has made it difficult to keep up with service level demands.



There are **over 150 bridge structures** throughout the City of Jackson including timber bridges, concrete bridges and concrete box culvert structures. The average life of the bridge structures currently in service is 35 years. All bridge structures are inspected every two years by the Mississippi Department of Transportation State Aid Division; however, in-house staff in the Engineering and Infrastructure Management Divisions regularly monitors bridge structures to assess safety concerns. This diligence and use of in-house expertise has resulted in removing bridges from service prior to collapse.



Recent results of the MDOT State Aid inspections show that **19% of the 150 bridge structures are in need of replacement or critical maintenance**. A sample of the results is shown in the following figure.



The sufficiency rating formula provides a method of evaluating highway bridge data by calculating four separate factors to obtain a numeric value which is indicative of bridge sufficiency to remain in service. The result of this method is a percentage in which 100 percent would represent an entirely sufficient bridge and zero percent would represent an entirely insufficient or deficient bridge. The formula considers the structural adequacy; functional obsolescence and level of service; and essentiality for public use.

Compliance Programs/Priorities

The Americans with Disabilities Act (ADA) law, specifically Titles II and III governs transportation and public accommodations for people with disabilities. The City is in the process of implementing an ADA Transition Plan to remove physical barriers to accessibility and incorporate ADA compliant features in right-of-way improvements including curb cuts and sidewalk modifications during roadway and sidewalk improvement projects.

The Federal Highway Administration (FHWA), which has oversight of surface transportation programs and sets minimum requirements and standards for transportation infrastructure, has established an asset management focus. To best service and maintain our Bridges, the Engineering Division will adopt the Systematic Preventive Maintenance Program (SPM) that is currently used by FHWA as part of our Department-wide Asset Management Initiative. The AASHTO Subcommittee on Maintenance's definition of "preventive maintenance" includes the phrase "a planned strategy of cost-effective treatments." An SPM program is based on a planned strategy that is equivalent to having a systematic process that defines the strategy, how it is planned, and how activities are determined to be cost effective. An SPM program for bridges can be defined as a planned strategy of cost-effective treatments to existing bridges that are intended to maintain or preserve the structural integrity and functionality of elements

and/or components, and retard future deterioration, thus maintaining or extending the useful life of the bridge.

Programmatic Initiatives

Given the magnitude of the investment needed to improve the condition of the City's streets, upgrade and innovate our signalization and bring the City into full compliance with ADA requirements the City recognizes the need to invest smartly. This means addressing the underlying issues of water and sewer utility failures, drainage issues and flooding that cause standing water. These chronic issues have led to severe degradation of the City's streets. DPW has therefore initiated a **DIG ONCE** philosophy requiring utilities are addressed before roadways are resurfaced including a street cut moratorium by third party utilities and curb to curb restoration of cuts to newly resurfaced streets. A review of past roadway resurfacing projects shows that resurfaced streets have been cut repeatedly due to utility failures. This is a major driver for using an integrated planning approach to identify all opportunities for infrastructure investment in a project location. This integrated planning approach will lead to implementation of **Complete Streets** and **Green Streets** projects. Implementation of the ADA Transition Plan is also a programmatic initiative.

While it is envisioned that major gateways, arterials and collector streets will be addressed as part of planned capital projects; DPW will initiate a renewed 5-year neighborhood streets resurfacing program called **Operation Orange Cone**. This program will be driven by condition assessment and pavement management data collected as part of the City's pavement management system, the number of utility cuts and other data to identify priorities for each Ward of the City. The street resurfacing program will be part of our new asset management initiative where we will be investing money wisely to extend the service life of our streets by using preventive maintenance measures before the roadway reaches a point of deterioration that requires a more sizable investment and planning. As additional data including funding availability are reviewed, goals for city-wide Street resurfacing and maintenance will be established. This program will be implemented using a combination of in-house crews and contract labor starting with the street listed in **Appendix B**. In-house crews will also continue to address pot hole related service requests; however, street resurfacing will address both potholes and street cuts.



In the past, street resurfacing has been prioritized utilizing the City's Pavement Management System. This system requires all streets to be evaluated every five years and assigned a priority rating from 0 to 1000. Streets rated 300 or below were considered for resurfacing provided there was adequate budget. Ratings under the current system for streets across the City are presented in the **Tables in Appendix A**.

Implementation of a systematic preventive maintenance program (SPM) will assist the City in qualifying for available Federal Funds for bridge replacement or rehabilitation. In addition to developing a formal bridge maintenance and asset management program, the 1% Sales Tax funds will be used toward implementation of a ***Jackson Safe Bridges program including timber bridge replacement***. This program was submitted as a Public-Private Partnership (P3) Program in response to the US Department of Transportation's Fall 2014 request for P3 pipeline projects and presents a way to leverage the limited revenues being generated by the sales tax.

Also under the 1% Sales Tax program, the Traffic Engineering section will initiate the “**Operation Green Light**” a comprehensive program to make upgrades to the City's signal network to compliment investments in rebuilding and resurfacing City streets.

The City of Jackson currently manages all Parking Meter services in-house. The Jackson Police Department and Municipal Courts are responsible for enforcement and the Public Works Department is responsible for the day-to-day management of the Parking Meter coin collections, reserved services and maintenance.



The Public Works Department is benchmarking new technology and best management practices used in other similar cities for investing in, managing and maintaining parking meters to improve upon the revenue generated from this service. Based on Infrastructure Management's review, there exists an opportunity to update the City's antiquated parking meter system with a more efficient and modern technology that will allow citizens to pay for parking using a mobile application, credit cards, license plate services, call in services, pay by phone, and pay stations. In 2015, PWD is making efforts to pilot new technology in the City to determine the scale of investment needed and the return on the investment.

The Public Works Department plans to decrease the amount of parking meters by adding more efficient meters that can account for more than one parking space; allowing the Department to make the maintenance efforts more efficient.

Historical Investments and Funding Sources

The City has typically funded roadway improvement projects with General Funds and grant funds including Community Development Block Grant Funds, funding from MDOT and STP funds through the CMPDD. The City had historically committed \$1M a year to street resurfacing; which could in no way keep up with the needed investment to resurface streets on a continuous basis. However, with the passage of the 1% sales tax, it was determined that the sales tax funds would be used as the primary funding source to free General Funds for other uses and get more mileage in the street resurfacing program.



Bridge projects have been largely funded by road bond funds, Local System Bridge Program (LSBP) funds and direct Federal appropriations. Recent investments have been made to address deficiencies of the South Street Bridge, Forest Hill Road bridge and the Brookwood Drive Bridge.

Investment Priorities

As evidenced by the number of service requests for potholes and utility cuts, roadway improvements in the City of Jackson is an investment priority for use of the 1% Sales tax funds. We understand that the condition of the roads in Jackson not only impact public perception of the City, but there are real impacts to vehicles traveling our city streets including service and emergency vehicles. The limited investment in upgrading the City's roads has been due to limited available funds. The deterioration of roads and the increasing number of utility cuts due to pipe failures has outpaced the production of in-house crews that are responsible for roadway maintenance because many of the roads are beyond a state of repair and instead need complete reconstruction. The investment priorities for our roads are as follows:

- Development of gateway improvement projects using Complete Streets or Green Streets concepts that incorporate integrated infrastructure improvements, roadway reconstruction where required, greening, utility work, signal upgrades, ADA compliance measures and right-of-way improvements to draw economic development and improve the vibrancy of communities
- Projects that address the 50 worst streets
- Projects where grant funding has been identified and allocated to the City for specific roadway projects
- **Operation Orange Cone:** Initiation of an in-house based roadway resurfacing program to improve condition of neighborhood streets. This will require investing in additional and upgraded capital equipment.
- **Operation Green Light:** Upgrades to the City's signal network utilizing new technology and implementing measures to sustain signals.
- New developments served by bridge or culvert structures
- All major arterial bridge crossings
- Replacement of all timber bridge structures

Water System Improvements

The City of Jackson supplies high quality drinking water to consumers in the Cities of Jackson and Byram. The distribution service area is approximately 150 square miles and provides potable water to about 174,000 people. The City's water supply system includes two water treatment plants, O.B. Curtis (50 MGD) and J.H. Fewell (32 MGD); one booster pump station; and sixteen elevated and ground storage tanks with a combined storage capacity of 21 million gallons. Drinking water is distributed through 881 miles of water mains ranging in size from 2-inches to 48-inches in diameter. Most of these mains are constructed of cast iron, but some of the larger mains are steel or reinforced concrete. Other system appurtenances include 4,000 valves and 7,000 fire hydrants.

State of existing assets

The ice storm of 2010 highlighted the vulnerabilities of the water system when mass water outages were experienced across the City, particularly in the Downtown area. While the City produces high quality water, an estimated 40% of the water we are paying to treat is unaccounted for or lost in water main leaks and breaks as well as leaking valves and hydrants. Consumers are experiencing discolored water due to build-up in the water mains that have been in service for 60 to 100 years. These issues impact the reliability of the existing system and investment in replacing water mains is needed to reduce water loss and improve water quality received at the tap. In some small areas of the City still have 2-inch to 4-inch diameter water mains, adequate fire protection is a concern; these areas are an investment priority for small diameter pipe replacement.

The water treatment plants and storage facilities are also in need of upgrades to maintain service levels; however, those improvements will not be included in the scope of the 1% sales tax program.

Compliance Programs/Priorities

Operation, maintenance and capital improvement of the water system is governed by the federal Safe Drinking Water Act and the Mississippi Safe Drinking Water Act of 1997 and Mississippi Primary Drinking Water Regulations. The City's Water and Wastewater Operation is responsible for ensuring that our system meets or exceeds required regulations, standards and the requirements of the State's Public Water Supply Program including identification and elimination of operational and maintenance problems that may impact water quality. Replacing water mains that are leaking and have build-up of deposits causing discolored water will aid in meeting this objective and cut down on the number and frequency of Boil Water Alerts that are issued.

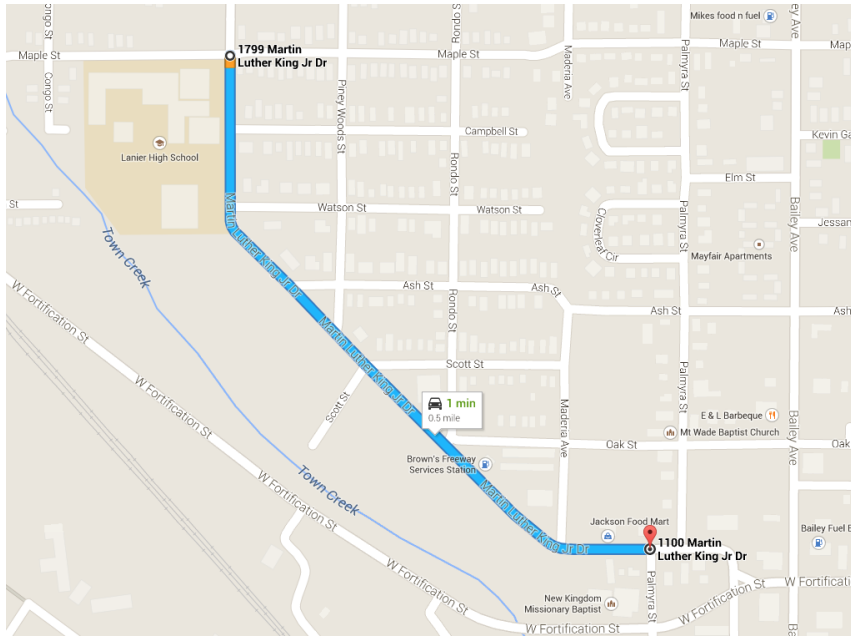
S 250008 (#1)

PRECAUTIONARY BOIL WATER NOTICE

Due to the recent loss in water pressure from a water main repair, the City of Jackson Water/Sewer Utilities Division has issued a precautionary boil water advisory until further notice for the following area:

- [1100-1799] Martin Luther King Jr. Drive; 39203

THIS ADVISORY AFFECTS APPROXIMATELY 15 CONNECTIONS ON OUR DRINKING WATER SYSTEM.



Programmatic Initiatives

Given the high percentage of water loss from the system and the repeated water outages; the water system is a primary target for the Department's Asset Management Initiative. In order to reduce water loss and eliminate water system failures, investment must be made in replacing water lines that are beyond their useful life. Under the scope of the 1% Sales Tax, a water line replacement program will be initiated starting with replacement of 2 to 4-inch water lines as well as cast iron pipeline replacement.

Additional programmatic initiatives include the Water Maintenance Division initiating an overall water system preventive maintenance program to:

- Assist the Fire Department with hydrant maintenance and asset management
- Valve exercising, maintenance and replacement
- Water system flushing and where possible water main cleaning and lining
- Water system audit including development of a water system hydraulic and water quality model
- Mapping the water system and related appurtenances

These initiatives may result in capital projects or in-house asset program management activities that would qualify for use of the 1% sales tax revenues.

Historical Investments and Funding Sources

The City funds water system improvements using water revenues generated from the water charges on consumers' water bills. Additional funding has been in the form of grants, loans or bond proceeds. While the 2013 water rate increase of 29% has generated additional revenue; it is just covering the cost of service for operation and maintenance and repayment of debt service on completed or on-going projects. Recent on-going and completed projects include:

- Water Infrastructure Improvement Program (Siemens) \$90M – On-going
- Prestressed Concrete Ground Storage Tank Contract 1 - \$2M
- 24” Water Transmission Main – Contract II - \$1M
- Maddox Booster Station – Contract III - \$2.2M
- Fortification Water Tank - \$4M
- 48” Water Transmission Improvements \$5M – On-going

Investment Priorities

Because the water system has a sustainable dedicated funding source, water system related projects recommended for funding by the 1% sales tax revenue are critical improvements to provide adequate fire protection and address emergencies by supplementing water funds. This will include upgrading 2 to 4-inch water lines and emergency repairs for unplanned failures.

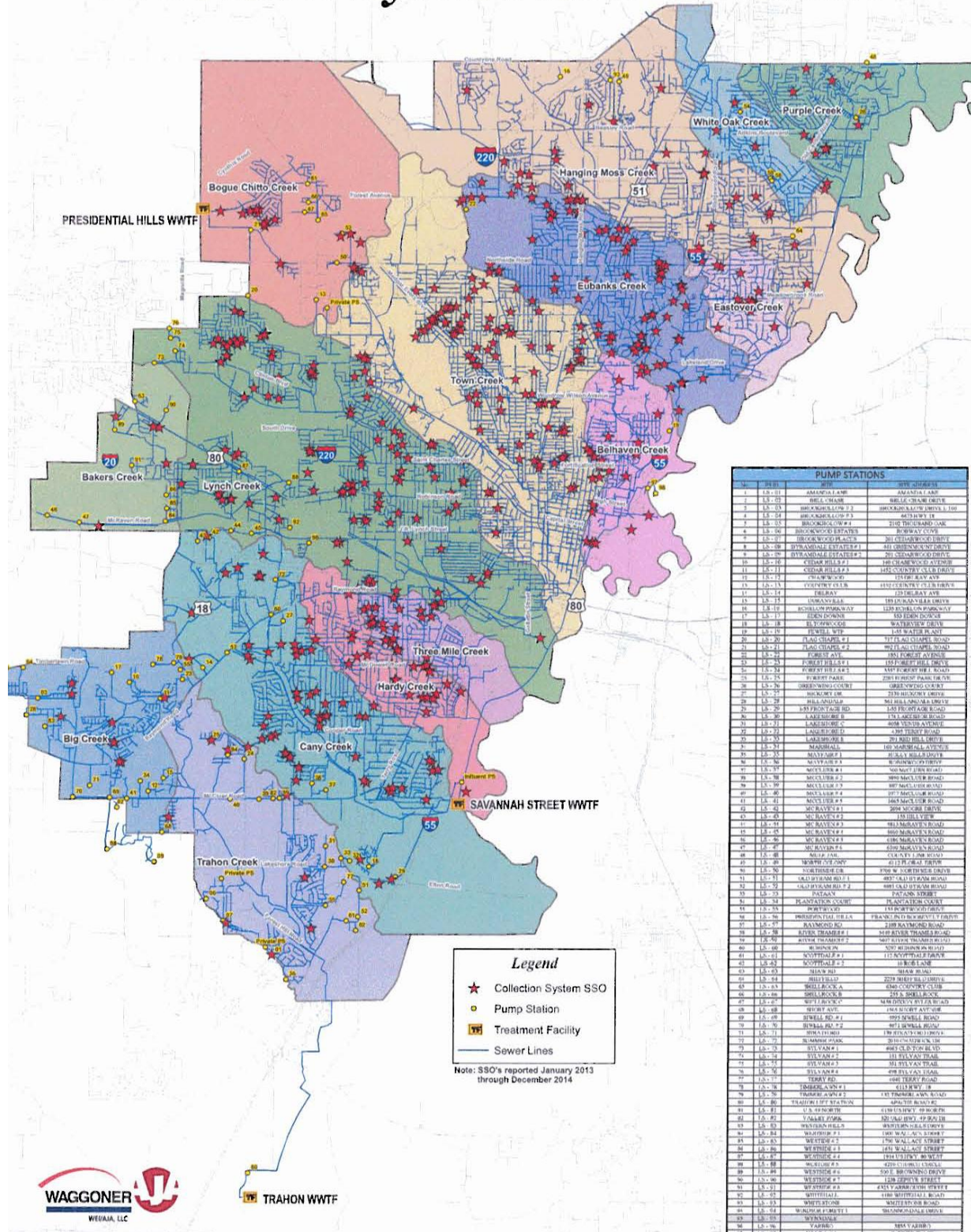
Sewer System Improvements

The City owns and operates three wastewater treatment facilities, plus numerous interceptor and collector sewers throughout the metropolitan area, and provides sewer services to the City and parts of Hinds County, western Rankin County and eastern Madison County. The primary wastewater treatment facility, Savanna Street Wastewater Treatment Plant (WWTP), has a 46 million gallons per day (MGD) permitted capacity and was placed in operation in 1975. The Trahon WWTP has a capacity of 4.5 MGD and treats flows from the Trahon and Big Creek Basins. The City’s third treatment facility is located in the Presidential Hills subdivision in the northwest part of the City. It was recently upgraded to a new facility providing 0.75 MGD of permitted treatment capacity. Wastewater flows to these facilities through a collection system with pipes ranging in size from 4-inches to 96-inches in diameter.

Compliance Programs/Priorities

Operation, maintenance and capital improvement of the wastewater collection system is governed by the federal Clean Water Act and the Mississippi Department of Environmental Quality WPC1 Regulations. In November 2012, the City reached a settlement with the U.S. Environmental Protection Agency (EPA), the Department of Justice, and the Mississippi Department of Environmental Quality (MDEQ) to make improvements to the City’s sewer systems to eliminate unauthorized overflows of untreated raw sewage (Sanitary Sewer Overflows or SSOs) and unauthorized bypasses of treatment at the Savanna Street Wastewater Treatment Plant (WWTP).

City of Jackson, MS Collection System SSO Location



The consent decree requires Jackson to implement comprehensive injunctive relief within approximately 18 years, with the majority of the work being done in the first 11 years of the consent decree program. The remedial measures required by the consent decree include:

- Early action projects to upgrade the Presidential Hills WWTP and make improvements to Savanna Street WWTP
- Evaluation and rehabilitation of the wastewater collection and transmission system
- Development and implementation of a performance evaluation and composite correction program for the Savanna Street wastewater treatment plant
- Development and implementation of programs to insure proper capacity, management, operations and maintenance of the sewer system

Programmatic Initiatives

The primary initiatives for the sewer system are implementation of the consent decree programs and asset management to maintain compliance and eliminate sanitary sewer overflows. The consent decree program projects will be considered in conjunction with the stormwater projects in an integrated plan designed to improve water quality through both gray and green infrastructure improvements.

Historical Investments and Funding Sources

The City funds wastewater system improvements using sewer revenues generated from the sewer charges on consumers' water bills. Additional funding has been in the form of grants, loans or bond proceeds. While the 2013 sewer rate increase of 108% has generated additional revenue; it is just covering the cost of service for operation and maintenance and repayment of debt service on completed or on-going projects. Recent on-going and completed projects include:

- WBI Flow Monitoring \$350K – On-going
- West Bank Interceptor Condition Assessment \$600K – On-going
- WCTS Flow Monitoring \$200K – On-going
- WWTF Influent Pump Station \$2.5M – On-going
- WWTP Comprehensive Performance \$700K – on-going
- Evaluation/Composite Correction Program \$1M – On-going
- WWTP Solids Removal Project \$10M – On-going
- Presidential Hills WWTF Improvements \$7M
- West Bank Interceptor Rehab Phase 3 \$12M – On-going
- West Bank Interceptor Rehab Phase 4 & 5 \$19M – On-going

Investment Priorities

Because the wastewater system has a sustainable dedicated funding source, and the recent rate increase, no sewer system projects have been planned as a part of 1% sales program at this time to reserve this funding source for critical drainage, roads, bridges and water line projects that have not had available funding. While no sewer system projects will be funded through the 1% sales tax program, sewer system

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projects are included in the City's comprehensive integrated planning process that is intended to ensure adequate funding of the City's infrastructure needs in all areas of drainage, roads, bridges and drinking water infrastructure.

Capital Plan

A Capital Improvement Plan (Program), or CIP, is a short-range plan, usually four to ten years, which identifies capital projects and equipment purchases, provides a planning schedule and identifies options for financing the plan. A CIP typically includes:

- Listing of capital projects or equipment
- Timetable for the planning, design and/or construction/completion of the project
- Projects ranked in order of priority
- Plan for financing the projects
- Justification for the project
- Explanation of expenses for the project

To facilitate planning for improvements to the City's infrastructure assets, a 5-year capital improvement plan will be used for the Infrastructure Master Plan which will coincide with the City's broader capital improvement plan development strategy for all of the City's capital improvement needs. The first year of the CIP represents the proposed capital budget for the current fiscal year. The remaining four years of the CIP serve as a project look ahead and financial plan for capital investments. Through the life of the 1% Sales Tax funded program, the CIP will be updated annually, at which time the schedule, prioritization funding sources and planned improvements will be re-evaluated and an additional fiscal year of projects will be added with new projects or project phases, as required.

To be considered eligible for 1% Sales Tax Funding, capital projects must be included in the Infrastructure Master Plan, contribute to the improvement of capital assets that are a part of the targeted infrastructure systems and fit one or more of the following categories:

1. Construction of new facilities or infrastructure.
2. Non-recurring rehabilitation or major repairs to a capital asset.
3. Projects requiring debt obligation or borrowing.
4. Purchase of major equipment for the purpose of asset management
5. Any specific planning, engineering study or design work related to a project that falls in the above categories.

Projects presented in this Infrastructure Master Plan may have one or more sources of funding; however, the City has included projects in this master plan that are considered UNFUNDED with the exception of project opportunities that have existing grant funding available where the City must provide a match.

Recognizing the 1% Sales Tax Revenue is insufficient to meet the total estimated \$1Billion in needed infrastructure improvements across the City of Jackson; a project prioritization scheme was developed to evaluate and rank the project opportunities for the Year 1 CIP (FY 2015). Additionally a funding strategy was developed to leverage the sales tax revenue in a manner that would not tie up future revenues in debt service payments.

Prioritization of Capital Projects

Proposed capital projects were scored using ten criteria to receive up to 100 points. This is to provide a measure of objectivity in the assessment of the relative priority of projects and resulting funding commitments. The goal of the project scores is to help rank projects within infrastructure system categories. For instance, projects whose major components include drainage improvements are ranked relative to each other. The evaluation criteria are listed in **Table X**. This fiscal year's project scores are shown in the Year 1 CIP Summary **Table X**. During the annual planning process, projects will be re-evaluated and rankings will be updated accordingly.

Table X – Project Evaluation Criteria

Health, Safety & Welfare	An assessment of the degree to which the project improves the health, safety and welfare of the community in which the project is sited, as well as improvements across the City. For example, projects that result in improved fire flows, reduction of accidents, improved structural integrity, and mitigation of health hazards and resolution of exigent circumstances would score higher.	20
Regulatory or Legal Mandates	An assessment of the degree to which the project is under a regulatory order or other legal mandate, or meets a federal, State or local regulatory requirement. For example, projects that are required by consent decrees, court orders, and other legal mandates would score higher.	20
Integrated Improvements	An assessment of the degree to which a project meets multiple infrastructure system improvement needs. For example, a roadway reconstruction project that addresses flooding issues, signals, ADA compliance needs and utility improvements will score higher than a project focused on street resurfacing.	10
Operational Improvements	An assessment of the degree to which the project supports operational efficiency and effective delivery of services. Guidelines: <i>Improves</i> operational functions and services: up to 10 points <i>Sustains</i> operational functions and services: up to 5 points	10
Implication of Deferred Project Implementation (opportunity costs)	An assessment of the costs associated with deferring the project, such as inflationary construction costs or additional annual operating and maintenance costs for each year the project is not funded. For example, projects that would have significantly higher future costs, negative community aspects, or negative public perception, should they be deferred, would score higher.	10
Budget Impact	An assessment of the degree to which a project affects operations and maintenance costs positively or negatively. For example, a pipe replacement project that reduces repeated emergency maintenance costs or storm drainage improvements that reduce the need for periodic cleaning would score higher. On the other hand, a new facility that increases maintenance, energy and staffing costs would score lower.	5
Triple Bottom Line Benefits	An assessment of the degree to which the project furthers the City's expressed triple bottom line benefits as discussed in the Master Plan Section.	6
External Funding	An assessment of the amount of funding in the project compared to the amount of	7

Opportunity	funding provided by grants or other external funds including public/private partnership financing opportunities. This will include an assessment of the amount of funding needed to complete the current project phase and the entire project. For example, a project that would bring grant funds from an outside agency into the City would score higher, while a project that relies only on City funds would score lower.	
Community Demand	An assessment of the degree to which the project meets a community need or responds to community demand. How need/demand is assessed, measured, or recorded will be noted including resolution of CSR complaints.	7
Shovel Ready/ Implementation Timeline	An assessment of the time required for a project to begin. This includes an assessment of: projects ready for construction; project complexity; internal decisions/commitments that are required; review requirements by boards/commissions; agreements or approvals required by non-City entities; timing considerations with other capital projects (if applicable) and public acceptance period. Whether a public information strategy is recommended will be noted.	5
		100

Project Requests

Project request forms are being used to compile information relative to each project being considered for funding. These request forms identify the project phase which allow for budget planning and forecasting long-term capital funding needs. The project phases include:

- **Project Planning:** Includes the development of the project scope, feasibility study, a design budget, and order of magnitude construction budget.
- **Design:** Includes the development of any environmental documents, legal documents, construction plans and specifications, and a detailed construction budget.
- **Construction:** Includes site preparation, utility and infrastructure placement, equipment installation, construction, and environmental mitigation.
- **Operation and Maintenance:** Includes routine and preventive maintenance programs for infrastructure assets.

Additionally, each project has a detailed project summary report which includes the project name, project manager, department responsible, project description, project justification including evaluation criteria, total estimated cost, sources of funding, fund number, fund name and the year in which funding will be needed or available.

Project Funding

As a rule of thumb, the City will prioritize the use of 1% Sales Tax revenue for project funding to those projects addressing infrastructure systems without a sustainable funding source, such as drainage, roads and bridges; over systems that have a dedicated funding source such as water and sewer. Projects with multiple elements may be funded using several funding sources depending upon the estimates of each element of work.

In order to address the magnitude of infrastructure improvement needs, the City will need to consider all sources of public and private financing available when developing and implementing the Bold New Infrastructure Improvement Program. Though the focus of this plan is leveraging the 1% Sales Tax

revenue, the City understands that in order to utilize the sales tax funds, the projects must be in this plan for approval, and in order to be implemented, the sales tax revenue must be supplemented with all other available funding sources. Other sources of funds may include: operating funds, General Obligation Bonds, Revenue Bonds, government loans and grants, Community Development Block Grant (CDBG) funds, revenue from fees/enterprise funds, private sources and contributions.

Fiscal Year 2015 Capital Plan

The City's Core Technical Group and project team developed the FY 2015 capital plan using an integrated planning approach to identify holistic solutions that would provide the maximum benefit to the community, improvements in health and safety as well as achievement of triple bottom line benefits. The primary objectives of the first year of capital improvements using the 1% Sales Tax revenue are:

- Fund shovel ready projects;
- Begin addressing severe drainage issues;
- Implement water system upgrades in areas with 2-inch to 4-inch water mains for improved fire flows;
- Establishing an in-house storm drainage system asset management program;
- Establish On-Call/Urgent Need contracts to address pressing concerns as they arise while long term projects are being planned and designed for construction in later years;
- Implement Dig-Once philosophy; addressing utility deficiencies with roadway repairs;
- Get major studies underway to comprehensively plan for future improvement needs in a manner that sustainably addresses issues; and
- Fund Program Management Services for continued project development, urgent need design, project delivery and development of the City's Green Infrastructure Strategy.

The process used to identify project opportunities included:

- Established an initial project planning horizon of \$50M
- Review of existing Master Plans and other planning level documents for each infrastructure system
- Data analysis: Customer Service Requests (CSRs), Street Cut data, Sanitary Sewer Overflow data, inspection data, water main break data, hydrant data
- Identification of Critical Assets
- Identification of on-going drainage and flooding issues
- Identification of roadway/bridge deficiencies
- Review of Water and Sewer system improvement needs
- Prioritizing based upon severity of issues, evaluation criteria, economic development potential and funding availability

Sales Tax Revenue Funding Strategy

A funding model was developed to run scenarios for the use of the Sales Tax revenues as a means to leverage debt or to use as PAYGO. As additional information on the availability of external funding sources is determined, the funding model will be updated year by year. The City will also need to have a

financial advisor to assist in developing more definitive debt financing scenarios. However, having identified an estimated \$60.1M in improvements for the FY 2015 CIP, the project team has developed a preliminary financing plan for the FY 2015 CIP.

Since the sewer system has a dedicated funding source, revenue from the sewer enterprise are anticipated to fund sewer projects identified in the Comprehensive CIP Plan. Water line improvement projects (\$4,188,000 in 2015) are proposed for the 2015 IMP CIP. These projects are not currently funded. Additional water system projects will be funded and financed using water revenues or alternative funding sources.

The projects for drainage, bridges, and streets which total \$56,393,000, will be funded through the Infrastructure Sales Tax revenue, ad valorem (tax) revenue, and/or available grants. The current funding plan assumes the entire balance will be funded through the Infrastructure Sales Tax directly and indirectly through the issuance of debt backed by the Infrastructure Sales Tax revenue in future years. The funding plan, **Table X** below, shows that \$9,000,000 will be directly funded by Infrastructure Sales Tax revenue, assuming the City will issue bonds to fund the remaining \$47,393,000. The assumptions for this bond issue include a 20-year term, 5.0% interest rates, and 2% issuance cost. Once the City has a financial advisor in place, a more definitive debt financing plan will be developed.

Table X – Preliminary Master Plan Funding

CIP 2015 through 2019					
	2015	2016	2017	2018	2019
Sources					
Sales Tax Revenue	13,624,608	13,760,854	13,898,463	14,037,447	14,177,822
Uses					
2015 Debt Service	3,878,996	3,878,996	3,878,996	3,878,996	3,878,996
Future Debt Service		-	-	-	-
Total	3,878,996	3,878,996	3,878,996	3,878,996	3,878,996
Direct Funding (PAYGO) Available	9,745,612	9,881,858	10,019,467	10,158,452	10,298,826
PAYGO Used	9,000,000				
Balance	745,612	10,627,471	20,646,938	30,805,389	41,104,215
Debt Funded	47,393,000	-	-	-	-
Annual Debt Service on Debt Funded	3,878,996	-	-	-	-
	5.0%	5.0%	5.0%	5.0%	5.0%
	20	19	18	17	16
	2.0%	2.0%	2.0%	2.0%	2.0%

The CIP Summary is presented in the following Table. **Appendix C** includes the detailed FY 2015 (Year 1) Capital Improvement Plan, including project descriptions and mapping.

Table X – Year 1 (FY 2015) Capital Improvement Plan Evaluation Criteria Scoring

Project Name	Ward	Total Project Cost	Health, Safety & Welfare	Regulatory or Legal Mandates	Integrated Improvements	Operational Improvements	Implication of Deferred Project Implementation (opportunity costs)	Budget Impact	Triple Bottom Line Benefits	External Funding Opportunity	Community Demand	Shovel Ready/ Implementation Timeline	Total
			20	20	10	10	10	5	6	7	7	5	100
Drainage													
Beechcrest Drive	1	\$ 1,044,000.00	16	8	5	7	5	3	4	3	5	3	59
North Canton Club	1	\$ 949,000.00	15	8	5	7	5	3	5	3	5	3	59
Woodhill Road to Beasley Road	2	\$ 1,848,000.00	17	8	5	7	5	3	4	3	5	3	60
George Washington Drive	2	\$ 1,848,000.00	19	8	5	7	5	3	5	3	5	3	63
Forest Avenue to Northside Drive	3	\$ 1,140,000.00	14	8	5	7	5	3	5	3	7	3	60
Woodrow Wilson to Erie Street	3	\$ 1,044,000.00	15	8	5	7	5	3	4	3	4	3	57
Parkway Dr. to Avenue D (Grove Park)	4	\$ 791,000.00	15	8	5	7	5	3	5	3	3	3	57
Brookwood Place	4	\$ 538,000.00	18	8	5	7	5	3	4	3	7	3	63
St. Charles Avenue to Robinson Road	5	\$ 1,232,000.00	17	8	5	7	5	3	4	3	5	3	60
Oakmont Drive to Primos Avenue	5	\$ 506,000.00	16	8	5	7	5	3	4	3	6	3	60
McDowell Road to Cooper Road	6	\$ 1,107,000.00	16	8	5	7	5	3	4	3	5	3	59
Lakeshore Road to Heritage Place	6	\$ 918,000.00	13	8	5	7	5	3	5	4	4	3	57
Belmont Street to Piedmont Street	7	\$ 949,000.00	19	8	5	7	5	3	5	4	7	3	66
Woodrow Wilson to Riverside Drive	7	\$ 759,000.00	17	8	5	7	5	3	5	4	7	3	64
Bridges													
Hanging Moss Road	2	\$ 633,000.00	18	10	5	5	4	4	4	3	4	3	60
Country Club Drive	4	\$ 345,000.00	19	10	5	5	4	4	4	3	4	3	61
Robinson Road	4,5	\$ 46,000.00	18	10	5	5	4	4	4	3	4	3	60
Alta Woods Boulevard	5	\$ 78,000.00	17	10	5	5	4	4	4	3	4	3	59

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Project Name	Ward	Total Project Cost	Health, Safety & Welfare	Regulatory or Legal Mandates	Integrated Improvements	Operational Improvements	Implication of Deferred Project Implementation (opportunity costs)	Budget Impact	Triple Bottom Line Benefits	External Funding Opportunity	Community Demand	Shovel Ready/ Implementation Timeline	Total
			20	20	10	10	10	5	6	7	7	5	100
Ready Mix Street 2	7	\$ 460,000.00	15	10	5	5	4	4	4	3	4	3	57
Streets/Roads													
County Line Road A-2	1	\$ 2,788,000.00	15	13	10	10	8	4	5	7	4	4	80
Medgar Evers Boulevard	3	\$ 7,000,000.00	15	13	10	8	6	3	5	4	4	4	72
State Street	3,7	\$ 3,452,000.00	18	14	10	8	10	5	5	4	6	3	83
Mill Street	3,7	\$ 1,812,000.00	16	10	8	5	5	4	5	3	4	3	63
John R. Lynch Street	5	\$ 1,135,000.00	14	12	9	8	8	4	4	6	6	4	75
Riverside Drive Phase 1	7	\$ 1,053,000.00	19	13	10	7	10	5	6	3	7	3	83
Riverside Drive Phase 2	7	\$ 548,000.00	19	13	10	7	10	5	6	3	7	3	83
Water Line Improvements													
Eastover Drive	1	\$ 1,388,000.00											
Lawrence Road	3	\$ 646,000.00											
McLeod Street	5	\$ 268,000.00											
Wooddell Drive Area	6	\$ 1,886,000.00											
Program Management		\$ 2,500,000.00											
Green Infrastructure		\$ 1,500,000.00											
Flex Funds		\$ 5,200,000.00											

Appendix A – Major Drainage Channel Improvements

Area	Drainage Channel/Tributary	Estimated Linear Footage (+/-)
Ward 1		
Adkins Blvd./Fairfax Circle	Tributary of White Oak Creek	800
North Canton Club	Tributary of Purple Creek	1000
Beechcrest	Purple Creek	1000
Ward 2		
Briarwood to Culley Drive	Tributary of Hanging Moss Creek	500
Knob Hill to Melba Hill	Tributary of Lynch Creek	1200
Winward/Whitestone to Winthrop	Tributary of Hanging Moss Creek Tributary No. 5.1	1000
Woodhill to Beasley Road	Tributary of Hanging Moss Creek Tributary No. 4.1	2000
Fontaine Park/Hallmark Drive (Wydette Hawkins)	Hanging Moss Creek	600
College Hill to Northside Drive	Bogue Chitto Tributary No. 5	750
George Washington Drive	Tributary of Bogue Chitto	2000
Presidential Drive	Tributary of Bogue Chitto	800
Ward 3		
Forest Avenue to Northside Drive	Eubanks Creek Tributary No. 6	1600
Woodrow Willson to Erie Street	Town Creek Tributary No. 4	1200
Ward 4		
Parkway Drive to Avenue D (Grove Park)	Town Creek Tributary No. 4	1000
Ward 5		
Oakmont Drive to Primos Avenue	Lynch Creek Tributary No. 3	600
St. Charles Avenue to Robinson Road	Lynch Creek	1500
Ward 6		
Lost Lake Cove	Trahan Creek Tributary	800
McDowell Road to Cooper Road	Hardy Creek	1600
Shirley Drive	Tributary of Cany Creek	750
Guinda Drive to Terry Road	Three Mile Creek	800
Lakeshore Road to Heritage Place	Tributary of Trahan Creek	800
Shiloh Drive	Tributary of Hardy Creek	300
Ward 7		
Belmont Street to Piedmont Street	Belhaven Creek	600
Woodrow Wilson to Riverside Drive	Belhaven Creek	800
Annexed Area		
Brookwood Place	Smith Creek	600

Appendix B – “Operation Orange Cone” Streets

Street Name	Priority Rating	From	To	Estimated Cost
WARD 1				
Clubview Dr.	200	Melrose Dr.	Cul de sac	\$22,282.27
Parkway Dr.	200	Clubview Dr.	Old Canton Rd.	\$32,528.87
Sedgewick Dr.	250	Sandlewood Pl.	Cul de sac	\$57,789.51
Crane Ridge Dr.	275	Ridgewood Rd.	Insurance Center Dr.	\$58,712.03
Crestview Dr.	275	Meadowbrook Rd.	Douglass Dr.	\$54,701.81
Douglas Dr.	275	Northeast Dr.	Ridgewood Rd.	\$157,650.55
Douglas Dr.	275	Ridgewood Rd.	Lake Cir.	\$133,581.77
Douglas Dr.	275	Northeast Dr.	Cul de sac	\$48,793.30
Eastwood Pl.	275	Douglass Dr.	Eastwood Dr.	\$39,218.52
Highland Cir	275	Crestview Dr.	Meadowbrook Rd.	\$35,779.17
Kristen Dr.	275	Mossline Dr.	Pear Orchard Rd.	\$129,518.24
Lakeside Dr.	275	Lakewood Dr.	Dead End	\$29,275.98
Lelia Dr.	275	Lakeland Dr.	Rigdewood Rd.	\$32,955.71
Northeast Dr.	275	Robert Dr.	Eastover Dr.	\$47,417.15
Pebble Lane	275	Twin Lakes Cir.	Cul de sac	\$86,742.78
Rebel Dr.	275	Northeast Dr.	Cul de sac	\$8,132.22
Roxbury Pl.	275	Roxbury Rd.	Cul de sac	\$18,648.16
Roxbury Rd.	275	.20 miles N. of Eastover	.08miles N.	\$46,188.41
Sleepy Hollow Dr.	275	Ridgewood Rd.	Eastover Dr.	\$51,459.77
Eastwood Dr.	280	Eastover Dr.	Douglass Dr.	\$87,548.27
Northeast Dr.	280	100'South of Rebel Dr.	Robert Dr.	\$34,803.31
Pinewood Dr.	280	Eastover Dr.	Douglass Dr.	\$39,522.57
Eastline Dr.	285	Eastover Dr.	Robert Dr.	\$48,602.69
Petit Bois St. (North)	285	Ridgewood Rd.	Ridgewood Rd.	\$114,283.03
Roxbury Rd.	285	.08 miles North	Meadowbrook Rd.	\$16,264.43
Twin Lakes Cir	285	Eastover Dr.	Eastover Dr.	\$106,252.37
Roxbury Rd.	290	Eastover Dr.	20miles North	\$46,188.41
Kristen Dr.	295	Round-About	Mossline Dr.	\$7,479.06
Amhearth St.	300	Meadow Oaks Park Dr.	Romany Dr.	\$34,371.31
Baxter Dr.	300	Springridge Dr.	Kirsten Dr.	\$117,586.69
Beeahcrest Ct.	300	Beechcrest Dr.	Cul de sac	\$57,357.51
Brook St.	300	Meadow Rd.	Northside Dr.	\$58,005.51

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Carolwood Dr.	300	Stanton Dr.	Briarwood Dr.	\$24,066.21
Charter Oak P.	300	River Thames Rd.	Runnymede	\$37,798.54
Chelesa Ct.	300	N. Canton Club Cir.	Cul de sac	\$22,986.21
Clubview Dr.	300	Colonial Cir.	South Park Dr.	\$15,773.92
Colonial Cir.	300	Colonial Cir.	Cul de sac	\$27,972.25
Courtney Cove	300	Kristen Dr.	Cul de sac	\$39,466.64
Crestview Pl.	300	Crestwood Dr.	Dead End	\$25,047.23
Dogwood Dr.	300	Meadowbrook Rd.	Lake Cir.	\$57,141.51
Dover Pl.	300	E.Sedgewick Ct.	Cul de sac	\$45,756.41
Foxboro Dr.	300	Dead End	River Rd	\$55,296.49
Hartline Dr.	300	Plantation Dr.	Tracewood Dr.	\$30,414.49
Henderson Cir.	300	Meadow Ridge Dr.	Meadowbrook Rd.	\$40,986.37
Kristen Dr.	300	Dale St.	Round -About	\$92,160.82
Lake land Cir.	300	Lakeland Dr.	Lakeland Dr.	\$68,310.62
Lake Trace Dr.	300	Kings Pl.	Northside Dr.	\$35,779.17
Lockwood Ave.	300	Forest Park Dr.	Ridgewood Rd.	\$36,432.33
Manila Dr.	300	Naples Rd.	Robinhood Dr.	\$52,696.76
Mossline Dr.	300	Dead End	County Line Rd.	\$164,318.97
Mossy Cove	300	Mossline Dr.	Mossline Dr.	\$56,112.29
North Hill Dr.	300	Sandlewood Pl.	Riverwood Dr.	\$39,357.35
Old Orchard Pl.	300	Martingale Dr.	Meadowbrook Rd.	\$156,354.55
Pickford Ln.	300	E. Sedgewick Ct.	Cul de sac	\$22,986.21
Pinevale St.	300	Highland Cir.	Crestwood Dr.	\$22,986.21
Radcliff St.	300	Romany Dr.	Meadow Oaks Park Dr.	\$23,202.21
Redfox Rd	300	River Thames Rd.	Reddoch Dr.	\$77,827.30
Ridgeover Pl.	300	Ridgewood Rd.	Circle grass island Trees	\$22,770.21
River Rd.N.	300	River Rd. N.	Deer Trail	\$23,418.21
River Cove	300	Cypress Trail	Cypress Cir	\$16,480.43
River Glen	300	Cypress Trail	Cul de sac	\$22,986.21
River Hills Dr.	300	Lelia Dr.	Lakeland Dr.	\$27,649.54
River Rd.	300	Deer Trail	Cul de sac	\$22,770.21
River Rd.	300	Dead End	River Road N.	\$46,188.41
River Thames Pl.	300	River Thames Rd.	Dead End	\$22,986.21
River Thames Rd.	300	Old Canton Rd.	Kaywood Dr.	\$144,356.57
Robert Dr.	300	Eastwood Dr.	Berkley Dr.	\$84,575.05
Romany Dr.	300	Cul de sac	Westbrook Rd.	\$85,329.76
Runnymede Rd.	300	Old Canton Rd.	Redfox Rd.	\$70,073.72
Sedgewick Dr.	300	Beechcrest Dr.	Westbrook Rd.	\$131,735.46
Simwood Pl.	300	Roxbury Rd.	Cul de sac	\$30,249.27

PUBLIC COMMENT DRAFT

Southerland St.	300	Lelia Dr.	Rigdeewood Rd.	\$29,817.27
Springridge Cir.	300	Pear Orchard Rd.	Pear Orchard Rd.	\$34,155.31
St. Richards Dr.	300	Spruce St.	Lynnwood Dr.	\$113,115.46
Stanton Dr.	300	Concord Dr.	Colonial Cir.	\$16,372.43
Stanton Dr.	300	Concord Dr.	Marblehead Dr.	\$196,255.77
Suffolk Dr.	300	Reddoch Dr.	Saratoya Dr.	\$66,003.05
Thackery Pl.	300	E. Sedgewick Ct.	Cul de sac	\$9,540.08
Tracewood Dr.	300	Pear Orchard Rd.	Kristen Dr.	\$86,526.78
Venetian Way N.	300	Venetian Way	Cul de sac	\$155,053.40
Venetian Way S.	300	Venetian Way	Cul de sac	\$203,086.83
Winchester St.	300	Ridgewood Rd.	Highland Terrace	\$23,418.21
Estimated Total Construction Cost Ward 1:				\$5,095,857.43
Street Name	Priority Rating	From	To	Estimated Cost
WARD 2				
Clay St.	200	Forest Ave Ext.	Dead End	\$ 148,000.00
Commerce Park Dr.	400	Lorenz Blvd. N.	Dead End	\$ 129,500.00
Grant St.	400	Brown St.	144 N. of Blue St.	\$16,650.00
Hamilton Blvd.	400	Livingston Rd.	County Line Rd.	\$79,550.00
Lakeover Rd.	400	159' E. of Livingston Rd.	Dead End	\$16,650.00
Meadowlane Dr.	400	Beginning of Curb/Gutter	Forest Ave.	\$68,450.00
Patann St.	400	Boling St.	Dead End	\$40,700.00
Pine Island Dr.	400	W of Watkins Dr.	239' W of Watkins Dr.	\$55,500.00
Red Oak St.	400	Gray Rd.	Cottonwood Dr.	\$20,350.00
Sayles and Dixon Rd.	400	Country Club Dr.	Dead End	\$20,350.00
Brown St.	450	County Line Rd.	Brown St.	\$14,800.00
Clinton Blvd.	450	C & G W. of Magnolia Rd.	Magnolia Rd.	\$37,000.00
Clinton Blvd.	450	John Hopkins Rd.	C&G west of Magnolia Rd.	\$27,750.00
Club Court	450	Hillsdale Dr.	Dead End	\$49,950.00
Cottonwood Dr.	450	Megar Evers Blvd.	Pin Oak St.	\$9,250.00
Executive Pl.	450	Bounds St.	W.Frontage Rd.	\$275,650.00
Gallant Dr.	450	N. Commerce Pl. Dr.	Dead End	\$22,200.00
George Washington Dr.	450	James Monroe Dr.	Abraham Lincoln Dr.	\$14,800.00
Gray Cir.	450	Red Oak St.	Dead End	\$38,850.00
Gray Rd.	450	Medgar Evers Blvd.	Medgar Evers Blvd.	\$ 7,400.00
Hilda Dr.	450	Forest Ave Ext.	3500'to City Limits	\$33,300.00

PUBLIC COMMENT DRAFT

Homewood Cir.	450	Hamilton Blvd.	Cul de sac	\$14,800.00
Kirkley Dr.	450	Hanging Moss Rd.	Forest Ave.	\$ 3,700.00
Lakeover Rd.	450	Livingston Rd.	159 E. of Livingston Rd.	\$ 5,550.00
McCary St.	450	Gray Rd.	Gray Cir.	\$25,900.00
N. Ashley Dr.	450	Forest Ave. Ext.	Dead End	\$62,900.00
N. Commerce PL.	450	Beasley Rd.	598' South	\$ 5,550.00
N. Commerce Pl.	450	598' South	Dead End	\$ -
Peter Quinn Dr.	450	County Club Rd.	E.' 1537 Club Dr. to C&G	\$11,100.00
Pin Oak St.	450	Red Oak St.	Cotton Wood Dr.	\$ 7,400.00
Truman Cir.	450	Harry S. Truman Dr.	Cul de sac	\$ 5,550.00
West Hill Dr.	450	Magnolia Rd.	Dead End	\$38,850.00
Presidential Dr.	475	Franklin D. Roosevelt	Dead End	\$59,200.00
Amblewood Dr.	500	Amblewood Dr.	Cul de sac	\$ 3,700.00
Azalea Cir.	500	Azalea Dr.	Berwood Dr.	\$27,750.00
Beasley Ct	500	Beasley Rd.	Cul de sac	\$90,650.00
Bedford Ln.	500	Northgate Blvd.	Squire Ln.	\$55,500.00
Begonia Ct.	500	Floral Dr.	Cul de sac	\$ 3,700.00
Berryhill Pl.	500	Valley Ridge Dr.	Cul de sac	\$74,000.00
Brown St.	500	82' W. of Railroad Tracks	State St.	\$16,650.00
Culley Dr.	500	Keele St.	I-55 Frontage Rd.	\$20,350.00
Cumberland Blvd.	500	Valley North Blvd.	Cul de sac	\$14,800.00
Delano Dr.	500	Highwood Dr.	West High Dr.	\$ 7,400.00
Echlelon Parkway	500	Watkins Dr.	County Line Rd.	\$11,100.00
Francis St.	500	Manhattan Rd.	Clair St.	\$42,550.00
Fontaine Park Dr.	500	Beasley Rd.	Dead End	\$22,200.00
Green Harbor Dr.	500	Watkins Dr.	235' West	\$18,500.00
Green Harbor Dr.	500	235' West	Pine Island Dr.	\$88,800.00
Heather Ln.	500	Locksley Dr.	Cul de sac	\$53,650.00
Highland Dr.	500	Where C & G Beg.	Dead End	\$42,550.00
Horton Ave.	500	67' N. of Briarwood	Fernwood Dr.	\$20,350.00
Inglewood Ave.	500	Hillcrest St.	White Oak St	\$40,700.00
Inwood Dr.	500	Post Oak Rd.	Dead End	\$68,450.00
James M. Davis Dr.	500	Highland Dr.	Sylvia Dr.	\$12,950.00
Johnson Pl.	500	Lyndon B. Johnson Dr.	Cul de sac	\$24,050.00
Kelly St.	500	County Line Rd.	Dead End	\$216,450.00
Marquis St.	500	164' W. of Galaxie Dr.	Kelle St.	\$44,400.00
Millwood Pl.	500	Valley North Blvd.	Cul de sac	\$44,400.00
Mimosa Dr.	500	Cedars of Lebanon Dr.	Crepe Myrtle Dr.	\$11,100.00

PUBLIC COMMENT DRAFT

N. Briar Pl.	500	Sun Valley Rd.	Cul de sac	\$14,800.00
Nordell Dr.	500	Kirkley Dr.	Nakoma Dr.	\$42,550.00
Neering Trail	500	Nisqually Rd.	Nisqually Rd.	\$16,650.00
Newburg Pl.	500	Somerset Dr.	Cul de sac	\$20,350.00
Pennswood Pl.	500	Valley North Blvd.	Cul de sac	\$20,350.00
Perkins Dr.	500	Kelly St.	Dead End	\$301,550.00
Piermont St.	500	Meadowlane Dr.	Witsell Rd	\$55,500.00
Pine Bay Dr.	500	Pine Island Dr.	228' N. of Forest Ave.	\$388,500.00
Pine Island Dr.	500	239' W. of Watkins Dr.	Lake of Pine Dr.	\$16,650.00
Presidential Dr.	500	Where C & G Beg.	Franklin D. Roosevelt Dr.	\$ 3,700.00
Queen Christina Ln.	500	Flag Chapel Rd.	Dead End	\$20,350.00
Queen Eleanor Ln.	500	West Hill Dr.	Dead End	\$ 9,250.00
Queen Melissa Ln.	500	Queen Christina Ln.	Dead End	\$55,500.00
Queen Park Cir.	500	Mattox St.	Queen Christina Ln.	\$25,900.00
Richview Pl.	500	Sun Valley Rd.	Cul de sac	\$81,400.00
Rock Glen Place	500	Valley Ridge Dr.	Cul de sac	\$18,500.00
Rutherford Dr.	500	Hanging Moss Rd.	New Curb and Gutter	\$ 9,250.00
Square Ln.	500	Northgate Blvd.	Fairfield Dr.	\$53,650.00
Stratford Pl.	500	Squire Ln.	Cul de sac	\$16,650.00
Sylvia Dr.	500	Beasley Rd.	James M. Davis Dr.	\$20,350.00
Tarryton Place	500	Valley North Blvd.	Cul de sac	\$11,100.00
Tennyson St.	500	Heather Ln.	Forest Ave.	\$38,850.00
Telfair Pl.	500	Valley North Blvd.	Cul de sac	\$ 1,850.00
Thornwood	500	Post Oak Rd.	Cul de sac	\$57,350.00
Upton Dr.	500	Northside Dr.	Richardson Dr.	\$48,100.00
Upton Dr.	500	Richardson Dr.	Dead End	\$ 5,550.00
Van Buren Rd.	500	George Washington Dr.	George Washington Dr.	\$27,750.00
Valley Ridge Dr.	500	Post Oak Rd.	Cul de sac	\$74,000.00
Village Dr.	500	New C/G	1040' To Nakoma Dr.	\$131,350.00
Vine St.	500	76'E. of State St.	Frontage Rd	\$20,535.00
Washington St.	500	981' S of County Line Rd.	Perkins St.	\$12,950.00
Wesley St.	500	County Line Rd.	Cul de sac	\$ 9,250.00
Whitehouse St.	500	Gray Dr.	Dead End	\$181,300.00
White Oak St.	500	86' E. State St.	Bonita St.	\$40,700.00
Total Estimated Cost in Ward 2:				\$4,696,040.00
WARD 3				

PUBLIC COMMENT DRAFT

Street Name	Priority Rating	From	To	Estimated Cost
Ave H	300	Parkway Ave.	Dead End	\$32,400.00
Ave. D	300	Parkway Ave.	Livingston Rd.	\$97,200.00
Bay St.	300	Ridgeway St.	Dead End	\$97,200.00
Britton Ave.	300	Wilshire Dr.	Northside Dr.	\$32,400.00
Cleary St.	300	Dalton St.	Dead End	\$181,440.00
Clover Hill	300	Dead End	Pocahontas Ave.	\$42,120.00
Cromwell St.	300	W. Ridgeway St	Miller Ave.	\$32,400.00
Douglass Ave.	300	Eminence Row	Northside Dr.	\$32,400.00
Edgewood Terrace	300	McWillie Dr.	I-55 S. Frontage Rd.	\$9,720.00
Edgewood Terrace	300	Manhattan Rd.	McWillie Dr.	\$32,400.00
Eminence Row	300	Livingston Rd.	Douglass Ave.	\$97,200.00
Eminence Row	300	Douglass Ave.	Bailey Ave.	\$32,400.00
Fontaine Ave.	300	Eminence Row	Mayes St.	\$32,400.00
Fontaine Ave.	300	Dead End	Eminence Row	\$64,800.00
Forest Ave.	300	El Paso St.	Hanging Moss	\$129,600.00
Greenfield Ave.	300	Skyline Dr.	Dead End	\$97,200.00
Hanging Moss Cir.	300	Hanging Moss Rd.	Beaverbrook Dr.	\$32,400.00
Iris Ave.	300	El Paso St.	Hanging Moss	\$64,800.00
Johnston St.	300	Sears St.	Livingston Rd.	\$64,800.00
Lampton Ave.	300	Northside Dr.	Eminence Row	\$129,600.00
Lampton Ave.	300	Eminence Row	Mayes St.	\$97,200.00
Launcelot Rd.	200	Church Hill Dr.	Manhattan Rd.	\$129,600.00
Lawrence Rd.	300	Manhattan Rd.	Durfrey St.	\$69,012.00
Londonderry Dr.	300	Robinhood Dr.	Launcelot Rd.	\$162,000.00
Londonderry Dr.	300	Northside Dr.	Robinhood Rd	\$97,200.00
Main St.	300	Ave. H	Ave. D	\$77,760.00
Manila Dr.	300	Northside Dr.	Robinhood Rd	\$64,800.00
Mayes St.	300	Livingston Rd.	Bailey Ave.	\$40,176.00
Meadowlane Dr.	300	Northside Dr.	Beaverbrook Dr.	\$64,800.00
Memphis St.	300	Fontaine Ave.	Eminence Row	\$64,800.00
Miller Ave.	300	Skyline Dr.	Dead End	\$113,400.00
Plaza St.	300	Essex Ave.	Iris Ave.	\$64,800.00
Robinhood Rd.	300	Chruch hill Dr.	N. State St.	\$64,800.00
Robinhood Rd.	300	Manhattan Rd.	Church hill Dr.	\$97,200.00
Rutledge Ave.	300	Skyline Dr.	Medgar Evers	\$90,720.00
Sears St.	300	Eminence Row	Mayes St.	\$64,800.00
Skyline Dr.	300	Ln. Ridgeway St.	Greenfield Ave.	\$97,200.00
Stover Ave.	300	Miller Ave.	Dead End	\$32,400.00

PUBLIC COMMENT DRAFT

Triangle Dr.	300	Northside Dr.	N. State St.	\$97,200.00
Total Construction Cost in Ward 3:				\$3,402,324.00
WARD 4				
Street Name	Priority Rating	From	To	Estimated Cost
Brookhollow Cir.	275	Brookhollow Dr.	Brookhollow Dr.	\$ 32,400.00
Johnson Ct.	275	Beach St.	Capitol St.	\$104,328.00
Hawthorne Ln.	285	Morson Rd.	Dead End	\$ 72,252.00
Old Siwell Rd.	285	Raymond Rd.	Siwell Rd.	\$ 69,012.00
Commercial Ave.	290	Reserve Dr.	Industrial Dr.	\$ 82,944.00
Copperwell Dr.	290	Raymond Rd.	Dead End	\$ 27,540.00
Gardenia St.	290	Peach St.	Westhaven Blvd.	\$ 49,248.00
Heloise St.	290	Capitol St.	Road of Remembrance	\$ 15,228.00
Henley St.	290	Norman St.	Dewey St.	\$ 85,860.00
Hollywood Ave.	290	D.E. @ Kelly Ave.	Summitt Dr.	\$101,412.00
Jackson Hwy	290	Robinson Rd.	Hwy 80	\$142,560.00
Lake Dr.	290	Hawthorne St.	Dixie Dr.	\$ 43,092.00
Laurel Oak Pl.	290	Laurel Oak Dr.	Cul de sac	\$ 17,172.00
Lois Cv.	290	Barrier Pl.	Cul de sac	\$ 31,752.00
Lurline Dr.	290	T.V. Rd.	Stillwood Dr.	\$ 12,960.00
McAdoo St.	290	Bullard St.	Sagamore St.	\$ 33,696.00
Michelle Ct.	290	Michael Clay Dr.	Cul de sac	\$ 18,468.00
Montgomery St.	290	Norman St.	Orrell St.	\$ 18,468.00
Princess Pine Dr.	290	Hanging Moss Rd.	Dead End	\$9,396.00
Redwood Ave.	290	Will-o-wood Blvd.	.22 miles S of Will-o-wood	\$ 13,608.00
S.Alabama Ave.	290	South Dr.	Capitol St.	\$116,640.00
Segura Ave.	290	Road of Remembrance	Rosslyn Ave.	\$ 25,920.00
Sharp St.	290	Petunia St.	Westhaven Blvd.	\$ 61,884.00
Summer Dr.	290	Summer Dr.	Dead End	\$ 64,476.00
Twelve Oak Cir.	290	Hwy18	Hwy18	\$ 17,172.00
Vanderbilt Ave.	290	Clinton Blvd.	Dead End	\$ 71,280.00
Veterans Blvd.	290	Dead End	South Dr.	\$184,680.00
Wynndike Cir.	290	Westhaven Blvd.	Dead End	\$ 59,616.00
Barwood Ct.	295	Brookhollow Dr.	Cul de sac	\$ 54,756.00
Berry St.	295	Gardenia St.	Gault St.	\$188,244.00
Brandywine Ct.	295	Ramada Cir.	Cul de sac	\$ 49,248.00
Caroline Dr.	295	Westhaven Blvd.	Regina St.	\$ 81,000.00

PUBLIC COMMENT DRAFT

Carson Ave.	295	Hollywood Ave.	Dead End	\$ 77,436.00
Deckard Dr.	295	Westhaven Blvd.	Dead End	\$ 76,464.00
Deckard Dr.	295	Wiggins Rd.	Peach St.	\$ 55,728.00
Deer Oak Pl.	295	Princess Pine Dr.	Laurel St.	\$ 79,704.00
Deer Oak Pl.	295	Deer Oak Pl.	Cul de sac	\$7,452.00
Draughn Dr.	295	Wiggins Rd.	Peach St.	\$ 43,092.00
Gault St.	295	Hwy 80	Dewey St.	\$ 65,772.00
Heloise St.	295	Road of Remembrance	Redwood Ave.	\$133,164.00
Kelly Ave.	295	Dead End North of Hollywood Ave.	Dead End of S Hollywood Ave	\$ 51,840.00
Kingswood Ave.	295	Brookhollow Dr.	Cul de sac	\$8,100.00
Lynch St.	295	Westhaven Blvd.	Morson Rd.	\$ 77,112.00
Montgomery St.	295	Norman St.	Wallace St.	\$ 67,392.00
Moore Dr.	295	Dead End	Dead End	\$ 17,172.00
Oak Glenn Pl.	295	Brookhollow Dr.	Cul de sac	\$ 32,400.00
O'Ferrell Ave.	295	Capital St.	Culbertson Ave.	\$ 90,072.00
Orrell St.	295	Yarborough St.	Montgomery Dr.	\$ 79,704.00
Pear St.	295	Gault St.	Dead End	\$ 32,400.00
Petunia St.	295	Gardenia St.	Sharp St.	\$ 90,072.00
Queen Eleanor Ln,	295	West Hill Dr.	Flag Chapel Rd.	\$155,520.00
Queen Melissa Ln.	295	Queen Christina Ln.	Dead End	\$ 97,200.00
Ramada Cir.	295	Siwell Rd.	Siwell Rd.	\$ 52,164.00
Redwood Ave.	295	Mt. Vernon Ave.	Rosslyn Ave.	\$ 46,008.00
Redwood Cir.	295	Dogwood Trail	Dogwood Trail	\$ 69,660.00
Regina St.	295	Hwy 80	Dead End	\$136,080.00
Reserve Dr.	295	Bullard St.	Commercial Ave.	\$ 22,032.00
Rifle Ave.	295	Wynndike Cir.	Dead End	\$ 32,400.00
Ritchey Dr.	295	Lindbergh Dr.	Holden St.	\$125,712.00
Rosslyn Ave.	295	Redwood Ave.	Weston St.	\$8,100.00
Shadow Cv.	295	Brookhollow Dr.	Cul de sac	\$ 49,248.00
St. Clair St.	295	Morson Rd.	Dead End	\$ 32,400.00
St. Francis St.	295	Caroline Dr.	Dead End	\$138,996.00
Still Creek Dr.	295	Lurchine Dr.	Dead End	\$ 26,892.00
Truman St.	295	Montgomery Dr.	Dead End	\$ 27,864.00
Tulane Ln.	295	Westhaven Blvd.	Dead End	\$ 49,248.00
Twisted Oak Dr.	295	Oak Creek Dr.	Hanging Moss	\$ 83,592.00
West hill Dr.	295	Magnolia Rd.	Queen Eleanor Ln	\$123,120.00
Whispering Oak Cv.	295	Brookhollow Dr.	Cul de sac	\$ 52,164.00

PUBLIC COMMENT DRAFT

Williams St.	295	Dewey St.	Wiggins Rd.	\$ 52,164.00
Will-o-Wood Blvd.	295	Pine Ln.	Oakridge Dr.	\$ 82,620.00
Wyndallwood Ct.	295	Wyndalwood Rd.	Cul de sac	\$ 27,216.00
Apple St.	300	Gault St.	Dead End	\$ 92,016.00
Army St.	300	Commercial Dr.	Dead End	\$ 43,092.00
Barrier Pl.	300	T.V. Rd.	Stillwood Dr.	\$ 96,552.00
Bent Oak Dr.	300	Oak Grove Ln.	Twisted Oak Dr.	\$ 81,000.00
Brookford Ct.	300	Brookhollow Dr.	Cul de sac	\$ 50,220.00
Brookhollow Dr.	300	Gresham Cv.	Dead End	\$9,072.00
Burch St.	300	Shaw Rd.	Dead End	\$ 52,164.00
Burton St.	300	Deckard Dr.	Tulane St.	\$ 81,000.00
Cabaniss Cir.	300	Hwy 80	Westhaven Blvd.	\$ 76,788.00
Cedar Pines Dr.	300	Brookhollow Dr.	Cul de sac	\$ 20,736.00
Chandler Dr.	300	Northside Dr.	Michael Clay Dr.	\$ 47,952.00
Cresham CV.	300	Brookhollow Dr.	Cul de sac	\$ 14,580.00
Curtis St.	300	Windsor Dr.	Meadowview St.	\$ 18,468.00
David Dr.	300	Tulane Ln.	Deckard Dr.	\$ 33,696.00
Delmonico Dr.	300	Peach St.	Wiggins Rd.	\$113,400.00
Dewey St.	300	William St.	Dead End	\$ 50,868.00
Dewey St.	300	Willie St.	Fairchild St.	\$ 81,000.00
Dixie Dr.	300	Morson Rd.	Whiting Rd.	\$ 89,100.00
Dogwood Trail	300	Raymond Rd.	Cottonwood Ln.	\$103,032.00
Dolphin Dr.	300	Wiggins Rd.	Turner St.	\$ 55,080.00
Eisenhower St.	300	Wainwright Ave.	Halsey St.	\$ 73,548.00
Elfin Ave.	300	Morson Rd.	Sylvian St.	\$ 12,312.00
Garden Dr.	300	Englewood St.	Englewood St.	\$ 73,548.00
Greenway Dr.	300	Maddox Rd.	Causey Dr.	\$ 76,788.00
Greenway Dr.	300	Causey Dr.	Hwy18	\$ 47,952.00
Greenway Dr.	300	Hwy 18	Robinson Rd. Ext	\$ 36,936.00
Hanging Oak Dr.	300	Princess Pine Dr.	Dead End	\$ 73,548.00
Hillandale Dr.	300	Western Hills Dr.	Dead End	\$111,456.00
Holden St.	300	Ritchey Dr.	Sewanee Dr.	\$ 67,392.00
Holden St.	300	Sewanee Dr.	Road of Remembrance	\$ 32,400.00
Jackson Hwy	300	Hwy 80	Morton Ave.	\$ 18,468.00
Kelly Ave.	300	Hancock St.	Liberty St.	\$ 69,336.00
Laurel Oak Dr.	300	Princess Pine Dr.	Dead End	\$ 33,696.00
Lindbergh Dr.	300	Byrd Dr.	Veterans Blvd.	\$ 43,092.00
Lindbergh Dr.	300	Robinson Rd.	Dead End	\$ 32,400.00

PUBLIC COMMENT DRAFT

Lowery Ln.	300	Tulane Ln.	Deckard Dr.	\$ 61,236.00
Luter Ln.	300	Deckard Dr.	Tulane Dr.	\$ 81,000.00
Markham St.	300	Marion Ave.	Ford Ave.	\$ 55,080.00
McRaven Rd.	300	Maddox Rd.	Greenway Dr.	\$208,980.00
Mikelle Rd.	300	Hwy18	Dead End	\$ 73,872.00
Montgomery St.	300	Wiggins Rd.	Dewey St.	\$ 87,804.00
Montgomery St.	300	Dewey St.	Wallace St.	\$ 27,216.00
Mt. Vernon Ave.	300	Sewanee Dr.	Sumner Dr.	\$259,200.00
Murhead Dr.	300	Country Club Dr.	Dead End	\$ 67,392.00
Nelson Dr.	300	Sharp St.	Williams Dr.	\$ 34,992.00
New Post Rd.	300	Siwell Rd.	Dogwood Trail	\$ 32,400.00
Noble St.	300	Faulk Blvd.	Dixon Rd.	\$135,108.00
Oak Groove Dr.	300	Oak Creek Dr.	Laurel St.	\$ 66,096.00
Old Spanish Trail	300	Western Hills Dr.	City Limits	\$159,408.00
Overlook Pl.	300	Hidden Valley Ln.	Dead End	\$269,892.00
Parkway Dr.	300	Overbrook Dr.	Utah St.	\$ 32,400.00
Patch Ave.	300	Nimitz Ave.	McCain Ave.	\$124,740.00
Pecan Cv.	300	Cottonwood Ln.	Cul de sac	\$ 92,016.00
Pine Cv.	300	Will-o-wood Blvd.	Cul de sac	\$ 18,468.00
Plum St.	300	Gault St.	Gardenia St.	\$ 45,360.00
Plummer Cir	300	Raymond Rd.	Maddox Rd.	\$173,664.00
Queen Esther Dr.	300	Queen Julianna Ln.	Queen Marie Ln.	\$ 67,392.00
Queens Joanna Ln.	300	Queen Victoria Ln.	Queen Victoria Ln.	\$ 20,736.00
Rushing Dr.	300	Clinton Blvd.	Dead End	\$194,400.00
Sagamore Dr.	300	Boiling St.	Broadview St.	\$ 25,920.00
Shelia Dr.	300	Oakmont Dr.	Primos Ave.	\$ 26,244.00
Speights St.	300	Riffle Ave.	Cul de sac	\$ 64,800.00
Summer Dr.	300	Summer Pl.	Hwy80	\$ 16,200.00
Summer Pl.	300	Morson Rd.	Dead End	\$ 33,048.00
Sundown Trail	300	Old Spanish Trail	City Limits	\$ 64,800.00
Sylvan Trail	300	Clinton Blvd.	City Limits	\$162,648.00
Sylvian St.	300	Summer Pl.	Dead End	\$129,600.00
Tiki Ln.	300	New Post Rd.	Raymond Rd.	\$ 32,400.00
Tuner St.	300	Peach St.	Dolphin Dr.	\$ 44,064.00
Van Winkle Park Dr.	300	Whiting Rd.	Dead End	\$ 34,992.00
W.Lane Dr.	300	Queen Mary Ln.	Dead End	\$ 72,900.00
Wallace St.	300	Fairchild St.	Willie St.	\$ 90,720.00
Westland Way	300	Deckard Dr.	Dead End	\$ 46,656.00

PUBLIC COMMENT DRAFT

Whippor-Will Dr.	300	Lynch St.	McRaven Rd.	\$ 21,384.00
Whitehaven Rd.	300	T.V. Rd.	Robinson Rd. Ext.	\$ 70,632.00
Williams Dr.	300	Wiggins Rd.	Nelson Dr.	\$ 82,944.00
Wynndike Cir.	300	Wynndike Rd.	Wynndike Cir.	\$ 39,204.00
Zepher Dr.	300	Hwy 80	Dead End	\$149,040.00
Total Construction Cost in Ward 5:				\$ 10,714,356.00

WARD 5

Street Name	Priority Rating	From	To	Estimated Cost
Calhoun St.	250	Capitol St.	Dead End	\$40,824.00
Broadmoor Ave.	275	Lexington Ave.	Dead End	\$ 6,480.00
Pinetree Dr.	285	Shady Pine Ln.	McDowell Rd.	\$67,392.00
Wilton St.	290	Raymond Rd.	Dead End	\$15,876.00
Hickory Dr.	400	Raymond Rd.	Vernon Dr.	\$58,320.00
Houston Ave.	400	St. Charles St.	Jayne Ave.	\$29,160.00
Lemly St.	400	Jayne Ave.	Officer Thomas Catchings Dr.	\$22,680.00
Oakhurst Dr.	400	Alta Woods Blvd.	Colonial Dr.	\$90,720.00
Paden St.	400	Raymond Rd.	Cummins St.	\$35,640.00
Shady Pine	400	Meadow Lark Dr.	Pine Tree Dr.	\$29,160.00
Valencia Ct.	400	Elaine St.	Dead End	\$ 106,920.00
Lemly St.	425	Jayne Ave.	Academy Dr.	\$38,880.00
Didlake Dr.	500	Robinson Rd.	Loflin St.	\$81,000.00
East Dr.	500	Raymond Rd.	Alta Woods Blvd.	\$38,880.00
Houston Ave.	500	Capitol St.	Jayne Ave.	\$48,600.00
Jones St.	500	Valley St	Dalton St.	\$38,880.00
Long St.	500	Prentiss St.	Bradley St.	\$ 162,000.00
Mayfair Ln.	500	Alta Woods Blvd.	Paden St.	\$84,240.00
Metairie Rd.	500	Robinson Rd.	Dead End	\$58,320.00
Pearl St.	500	Dalton St.	Prentiss St.	\$45,360.00
School Cir.	500	Harley St.	Hwy 80	\$6,480.00
Valley St.	500	Capitol St.	Clinton Ave.	\$ 129,600.00
Fernley St.	525	Yarbro St.	Dead End	\$71,280.00
Oakhurst Dr. S/W	550	Shady Cir. s/w	Carmel Ave.	\$93,960.00
Wynwood Dr.	550	Robinson Rd.	Duane St.	\$81,000.00
Total Construction Cost: in Ward 5				\$ 2,799,360.00

WARD 6

Street Name	Priority Rating	From	To	Estimated Cost
Brooks Cv.	250	Brookwood Pl.	S. end of street	\$4,949.82

PUBLIC COMMENT DRAFT

Dianne Cv	250	Brookwood Pl	Dead End	
Catalina Cir.	275	Garden Park Dr.	Garden Park Dr.	\$117,059.20
Laurie Ln.	275	Wildwood Dr.	Dead End	\$9,457.28
Lost Lake Cv.	275	Cul de sac	Cul de sac	\$29,597.66
Middle Dr.	275	Dees St.	Dead End	\$853.88
Myrtlewood Dr.	275	Combs St.	Dead End	\$54,102.00
Chelsea Dr.	280	Marlendo Dr.	Friars Dr.	\$48,044.23
Fleetwood St.	280	Woodville Dr.	Smallwood Dr.	\$71,487.07
Lea Cir.	280	McDowell Rd.	McDowell Rd.	\$82,178.71
Poole St.	280	Terry Rd.	Dead End	\$1,024.66
Reno Dr.	280	Woody Dr.	Dead End	\$37,797.64
Scanlon Dr.	280	Charmwood Dr.	Raymond Rd.	\$154,149.79
Shannon St.	280	McDowell Rd.	Dead End	\$40,590.88
Southaven Dr.	280	Brookwood Pl.	S. end of street	
Wildwood Cir.	280	Meadow Ln.	Meadow Ln.	\$36,470.37
Woodview Dr.	280	Wooddell Dr.	Woody Dr.	\$51,742.58
Woodview Dr.	280	Woody Dr.	Smallwood St.	\$59,598.30
Anna Lisa Dr.	285	Castle Hill Dr.	Scanlon Dr.	\$57,288.03
Brookwood Lake Pl.	285	Brookwood Pl.	S. end of street	
Chelsea Dr.	285	Friars Cv.	Rainey Rd.	\$66,944.41
Diviney Cove	285	Brookwood Pl.	E. end of street	
Dorgan St.	285	Castle Hill Dr.	Dead End	\$107,816.03
Dovewood Dr.	285	Woodell Dr.	Woody Dr.	\$17,236.15
Evans Rd	285	Terry Road	W Frontage Rd	
Garden park Dr.	285	Shady Ln Dr.	Catalina Cir.	\$38,592.79
Killarney St.	285	Shannon St.	McDowell Rd.	\$19,237.67
McDowell Ct.	285	McDowell Rd.	McDowell Rd.	\$5,946.47
Scots Glen	285	Scanlon dr.	Dead End	\$52,020.00
Tivoli Terrance	285	Garden Park Dr.	Dead End	\$15,084.36
Autumn St.	290	Bienville Dr.	Suncrest Dr.	\$39,505.41
Bass Ave.	290	Puckett Rd.	W. Lake Rd.	\$16,257.93
Cherrie Ave.	290	Ronald Rd.	Dead End	\$8,065.44
Cooks Ave.	290	Ronald Rd.	Dead End	\$7,104.30
Cypress Dr.	290	Raymond Rd.	Will-o-wood Blvd.	\$67,454.03
Emerald Dr.	290	Shannon St.	Killarney St.	\$25,471.32
Fairhill Dr.	290	Woodbine St.	Fairhill Dr.	\$31,646.98
Gunda St.	290	Montery St.	McDowell Rd.	\$103,373.13
Holly Hill Dr.	290	Old Lake Rd.	Mayfair Dr.	\$57,780.77

PUBLIC COMMENT DRAFT

Leavell Woods Dr.	290	Woody Dr.	Cooper Rd.	\$35,742.91
Lost Lake Way	290	Lost Lake Cir.	Dead End	\$28,779.28
Maria Dr.	290	Dorgan St.	Joanne St.	\$75,990.15
Maria Dr.	290	Woodside Dr.	McDowell Rd.	\$89,737.66
McDowell Cir.	290	McDowell Rd.	McDowell Rd.	\$58,478.68
McDowell Cir.	290	McDowell Rd.	McDowell Rd.	\$13,064.40
Old Lake Rd.	290	Lakeshore Rd.	Holly Hill Dr.	\$140,890.65
Puckett Rd.	290	Cooks Ave.	May Ave.	\$16,701.95
Sloane	290	Breckinridge Rd.	Dorgan St.	\$32,871.78
Swan Hill Dr.	290	Rainey Rd.	Swan Ridge Dr	\$37,797.64
Swan Ridge Dr.	290	Rainey Rd.	Swan Lake Dr.	\$30,966.58
Sycamore Pl.	290	Sycamore Dr.	Dead End	\$12,193.45
Twin Oaks	290	Sycamore Dr. (Dead End)	Cedar Springs	\$47,502.54
Wingfield Dr.	290	Scanlon Dr.	Monaco St.	\$96,315.26
Wooddell Dr.	290	Cliffwood Dr.	Smallwood St.	\$69,644.76
Alyssia Ln.	295	Heritage Pl.	Dead End	\$10,408.83
Barberry Ave.	295	Wisteria Dr.	Camellia Dr.	\$20,873.69
Bracken Cv.	295	Brookwood Pl.	S. end of street	\$24,162.18
Brookwood Estates	295	Brookwood Pl.	W. end of street	
Camel Back Ln.	295	Mayfair Dr.	Dead End	\$6,118.59
Cooks Ave.	295	Ronald Rd.	Puckett Rd.	\$13,431.58
E. Woodcrest Dr.	295	Woody Dr.	Dead End	\$44,421.06
Fairhill Dr.	295	McDowell Rd.	Pinebrook Dr.	\$68,373.51
Freemont St.	295	Belvedere Dr.	Key St.	\$37,684.24
Harwen Pl.	295	Mayfair Dr.	Dead End	\$8,641.29
Haynes St.	295	Oak Forest Dr.	Corey Dr.	\$13,726.68
Heathway Cv.	295	Woodlee Dr.	N. end of street	
Lost Lake Cir.	295	Rainey Rd.	Rainey Rd.	\$106,433.46
Marshall Dr.	295	McClure Rd.	Dead End	\$40,587.45
May Ave.	295	Puckett Rd.	W.Lake Rd.	\$14,106.14
Old Poplar Rd.	295	Baybury Ln.	Holly Hill Dr.	\$43,599.26
Peacock Dr	295	Forest Hill Rd	Dead End	
Shady Brook Cv.	295	Woodlee Dr.	N. end of street	
South Haven Cir.	295	McDowell Rd.	McDowell Rd.	\$64,382.76
Tiffany Ln.	295	Heritage Pl.	Dead End	\$25,517.85
Treehaven Dr.	295	Greenview Dr.	Terry Rd.	
Will-o-Run Dr.	295	Will-o-Wood Blvd.	Sharon Hills Dr.	\$91,570.38
Wooddell Dr.	295	Smallwood St.	Meadow Forest Dr.	\$60,278.70

PUBLIC COMMENT DRAFT

Woodlee Dr.	295	Brookwood Pl.	W. end of street	
Alameda Dr.	300	Woody Dr.	Santa Clair St.	\$34,652.64
Anna Lisa Dr.	300	Shady Ln Dr.	Castle Hill Dr.	\$26,070.08
Aspin Cv.	300	Will-o- Lake Dr.	Dead End	\$14,140.30
Barbara Ann Dr.	300	McDowell Rd.	Maria Dr.	\$40,639.41
Barwood Dr.	300	Fairchild St.	Woodbine St.	\$38,878.32
Bass Ave.	300	Lake Rd.	Harold Rd.	\$7,040.79
Boozar Rd.	300	Mc Clure Rd.	Dead End	\$48,537.83
Breckinridge Rd.	300	Charmwood Dr.	Castle Hill Dr.	\$46,106.96
Briar Vista	300	Sharon Hills Dr.	Dead End	\$9,819.65
Briar Vista	300	Woodcliff St.	Dead End	\$19,639.31
Briarcliff Cir.	300	Marlendo Dr.	Spryfield Dr.	\$55,455.94
Carleton St.	300	Revere St.	Tara Rd.	\$20,493.19
Carleton St.	300	Donnybrook St.	Tara Rd.	\$37,341.33
Casa Visa Ct.	300	Old Lake Rd.	Dead End	\$15,801.62
Castle Cv.	300	Cypress Dr.	Dead End	\$16,165.04
Castle Hill Dr.	300	Raymond Rd.	Joanne St.	\$318,242.62
Catalina Dr.	300	Catalina Cir.	Valencia St.	\$10,246.59
Cedar Pl.	300	Will-o-lake Dr.	Dead End	\$15,533.08
Cherrie Ave.	300	Harold Rd.	Ronald Rd	\$6,916.46
Cliffwood Dr.	300	McDowell Rd.	Wooddell Dr.	\$51,938.99
Combs St.	300	Belvedere Dr.	McFadden St.	\$100,891.46
Del Rey Dr.	300	Plummer Cir.	Raymond Rd.	\$51,327.94
Diane Dr.	300	Maria Dr.	Flowers Dr.	\$40,185.81
Diane Dr.	300	Flowers Dr.	Maria Dr.	\$120,115.63
Dorgan St.	300	McDowell Rd.	Castle Hill Dr.	\$193,393.61
E. Lake Cir.	300	Sandlewood Dr.	Dead End	\$6,677.36
Engleside Dr.	300	McDowell Rd.	Dead End	\$22,238.64
Exchange Dr.	300	Dona Ave.	Cooper Rd.	\$27,665.80
Greenway Ct.	300	McDowell Rd.	Dead End	\$10,242.84
Harold Ave.	300	May Ave.	Bass Ave.	\$5,430.69
Harold Ave.	300	Taylor Ave.	Cherrie Ave.	\$12,494.02
Harrisdale Ln.	300	Brookwood Dr.	McDowell Rd.	\$10,564.92
Kee Ct.	300	Vineland Dr.	Dead End	\$22,265.51
Key St.	300	McDowell Rd.	Dead End	\$55,179.99
Lake Catherine Dr.	300	Lakeshore Rd.	Dead End	\$68,310.62
Lake Rd.	300	Cooks Ave.	Bass Ave.	\$12,295.91
Lake Rd.	300	Bass Ave.	May Ave.(dead end)	\$18,943.91

PUBLIC COMMENT DRAFT

W. Lake Rd.	300	Summit Ave.	Taylor Ave.	\$8,731.47
Lakeview Ave.	300	Puckett Rd.	W. Lake Rd.	\$17,128.89
LeavellWoods Dr.	300	Terry Rd.	Leavellwoods Dr.	\$70,752.10
Maria Dr.	300	JoanneSt.	Woodside Dr.	\$69,608.63
Marlendo Dr.	300	Briarcliff Cir.	Spryfield Dr.	\$25,216.20
Matt Cir.	300	Trent Dr.	Dead End	\$20,800.58
May Ave.	300	Lake Rd.	Harold Rd.	\$8,197.28
Mayfair Dr.	300	Terry Rd.	Old Lake Rd.	\$79,962.37
McDowell Cir.	300	McDowell Rd.	McDowell Rd.	\$100,630.50
Meadow Forest	300	Woody Dr.	Wooddell Dr.	\$40,424.87
Monaco Ct.	300	Monaco Dr.	Dead End	\$17,739.07
New Post Rd.	300	Spencer Rd.	Dogwood Trail	\$23,908.72
Oleander Ave.	300	Camellia Ln.	Wisteria St.	\$19,097.61
Puckett Rd.	300	May Ave.	Dead End	\$7,684.94
Reaves St.	300	Gunda St.	Mulberry St.	\$27,721.83
Rebel Cir.	300	Maddox Rd.	Forest Hill Rd.	\$29,580.58
Red Oak Dr.	300	Twin Oaks	Sycamore Dr.	\$128,309.21
Ronald Rd,	300	McCluer Rd.	Cooks Ave.	\$5,350.10
Russ Dr.	300	Syramore St.	Dead End	\$7,172.62
Sandlewood Dr.	300	Will-o-run Dr.	Beachwood Dr.	\$23,908.72
Shirley Dr.	300	Woody Dr.	Charleston Dr.	\$46,963.55
Spencer Dr.	300	Raymond Rd.	Will-o-Run Dr.	\$47,817.43
Summit Ave.	300	Ronald Rd.	West Lake Rd.	\$9,636.59
Summit Ave.	300	Ronald Rd.	Harold Rd.	\$9,523.19
Suncrest Dr.	300	Cooper Rd.	Smallwood St.	\$156,588.56
Suncrest Dr.	300	Smallwood St.	McDowell Rd.	\$80,031.41
Sundew Pl.	300	Redwood Dr.	Dead End	\$21,347.07
Swan Lake Cv.	300	Swan Lake Dr.	Dead End	\$15,369.89
Tanner Hill Dr.	300	McClure Rd.	Dead End	\$71,043.04
Taylor Ave.	300	West Lake Rd.	Ronald Rd	\$8,197.28
Teal Dr.	300	Canary Dr.	Peacock Dr.	
Trent Dr.	300	Vineland Dr.(Dead End)	Sycamore Dr.	\$81,516.44
Vinee Ct.	300	Old Lake Rd.	Dead End	\$9,243.80
Vineland Dr.	300	Sycamore Dr.	Trent Dr.	\$80,147.52
W.Lake Rd.	300	Summit Ave.	Taylor Ave.	\$8,731.47
Waxwing Pl.	300	Sharon Hills Dr.	Woodcliff St.	\$57,380.91
Waywood Dr.	300	Raymond Rd.	Cedarwood Dr.	\$50,865.79
Westville St.	300	Cedarwood Dr.	Dead End	\$11,954.36

PUBLIC COMMENT DRAFT

Will-o-Lake Dr.	300	Will-o-run Dr.	Dead End	\$52,599.17
Winn St.	300	McFadden Rd.	Gunda St.	\$59,580.19
Winn St.	300	Gunda St.	Belvedere St.	\$54,711.39
Woodcliff	300	Sharon Hills Dr.	Cedarwood Dr.	\$37,903.86
Wooddell Dr.	300	McDowell Rd. Ext.	Cliffwood Dr.	\$37,995.08
Woodell Ct.	300	Woodell Dr.	Dead End	\$10,268.46
Woodgrove Cir.	300	Sandlewood Dr.	Dead End	\$12,372.76
Total Construction Cost in Ward 6:				\$6,412,164.25
WARD 7				
Street Name	Priority Rating	From	To	Estimated Cost
Francis St.	150	West St.	Dead End	\$12,636.00
Lamar Ln.	150	Lamar St.	Dead End	\$55,080.00
Beatty St.	200	S West St.	Gallatin St.	\$129,600.00
Davis St.	200	Pleasant St.	Wood St.	\$22,680.00
Edgewood St.	200	Belmont St.	Euclid St.	\$32,400.00
George St.	200	High St.	Lamar St.	\$ 9,720.00
Wood St.	200	402' South of Cohea	Fortification St.	\$32,400.00
Arrowhead Dr.	250	Council Cir	Kings Hwy.	\$97,200.00
Mayes St.	250	Lawson St.	Bailey Ave. Ext. E/S	\$136,080.00
Park Dr.	250	State St. W/Bound	667' to Millsaps College	\$42,120.00
Tyrone Dr.	250	Old Canton Rd.	Oakridge Dr.	\$25,920.00
Congress St.	280	Fortification St. S/S	Parking Entrance Baptist	\$32,400.00
Luthers Cir.	290	Linde Rd.	Linde Rd.	\$45,036.00
Galloway Ave.	295	Old Canton Rd.	Seneca Ave.	\$38,880.00
Bellevue Pl.	300	Monroe St.	Quinn St.	\$42,120.00
Carlisle Dr.	300	Whitworth St.	Monroe St.	\$129,600.00
Carlisle St.	300	N. State St.	Whitworth St.	\$42,120.00
Cheston Ave.	300	Eddy St.	Afton St.	\$ 9,720.00
Commerce St.	300	Court St.	10' N of Tombigbee St.	\$19,440.00
Crestwood St.	300	Arbor Hill Dr.	Kingswood Ave.	\$61,560.00
Donald St.	300	N. West St.	Downing St.	\$113,400.00
Elton Rd.	300	I-55	Terry Rd.	\$113,400.00
Grandview Cir.	300	Terry Rd.	Grandview Cir.	\$136,080.00
Hamilton St.	300	Mill St.	Farish St.	\$58,320.00
Hazel St.	300	Euclid St.	Poplar St.	\$16,848.00
Judy St.	300	Lee St.	Camilla St.	\$45,360.00

PUBLIC COMMENT DRAFT

Julienne St.	300	Gallatin St.	Dead End	\$87,480.00
Lasalle St.	300	Abor Hill Dr.	Conner Ave.	\$ 9,396.00
Lee Dr.	300	Savanna St.	Bowers St.	\$48,600.00
Lee St.	300	Judy St.	Cameron St.	\$40,500.00
McDavid Ln.	300	Illinois Ave.		\$48,600.00
Mississippi St.	300	210'E of Jefferson St.	New Gate of Fairgrounds	\$16,524.00
Northview Dr.	300	Mayes St.	Hartfield St.	\$14,904.00
Peabody St.	300	Winter St.	Evergreen St.	\$21,060.00
Peabody St.	300	Winter St.	Porter St.	\$51,840.00
Pine Crest St.	300	Terry Rd.	Leaside St.	\$64,800.00
Railroad Ave.	300	Gallatin St.	Guidici St.	\$32,400.00
Rankin St.	300	232' W. Roach St.	Gallatin St.	\$35,316.00
Rio St.	300	Spengler St.	Dead End	\$45,360.00
Rio St.	300	High St.	Spengler St.	\$45,360.00
Riverside Dr.	300	Peachtree St.	Highland Dr.	\$81,000.00
Spengler St.	300	North Jefferson St.	Harding St.	\$35,640.00
St. Ann St.	300	Riverside Dr.	Dead End	\$51,840.00
Wagwood St.	300	Terry Rd.	Frontage Rd.	\$12,636.00
Waynut St.	300	Poter St.	Hooker St.	\$15,876.00
Wesley Ave.	300	Lamar St.	N. West St.	\$ 136,080.00
Wesley Ave.	300	N. Mill St.	Willow St.	\$ -
Wilmington St.	300	Terry Rd.	Dead End	\$32,400.00
Winter St.	300	Gallatin St.	Terry Rd.	\$32,400.00
Wood St.	300	Maple St.	Peach Orchard St	\$4,860.00
Woodland Hills Pl.	300	N.State St.	Dead End	\$25,920.00
Total Construction Cost in Ward 7:				\$3,297,348.00

Appendix C – Year 1 (2015) CIP

City of Jackson, Mississippi
Capital Improvement Plan Projects
2015 thru 2019

TABLE OF CONTENTS								
Project Name	Ward	2015	2016	2017	2018	2019	Total	Page
Drainage								8
Beechcrest Drive	1	\$ 1,044,000.00					\$ 1,044,000.00	10
North Canton Club	1	\$ 949,000.00					\$ 949,000.00	11
Woodhill Road to Beasley Road	2	\$ 1,848,000.00					\$ 1,848,000.00	12
George Washington Drive	2	\$ 1,848,000.00					\$ 1,848,000.00	13
Forest Avenue to Northside Drive	3	\$ 1,140,000.00					\$ 1,140,000.00	14
Woodrow Wilson to Erie Street	3	\$ 1,044,000.00					\$ 1,044,000.00	15
Parkway Dr. to Avenue D (Grove Park	4	\$ 791,000.00					\$ 791,000.00	16
Brookwood Place	4	\$ 538,000.00					\$ 538,000.00	17
St. Charles Avenue to Robinson Road	5	\$ 1,232,000.00					\$ 1,232,000.00	18
Oakmont Drive to Primos Avenue	5	\$ 506,000.00					\$ 506,000.00	19
McDowell Road to Cooper Road	6	\$ 1,107,000.00					\$ 1,107,000.00	20
Lakeshore Road to Heritage Place	6	\$ 918,000.00					\$ 918,000.00	21
Belmont Street to Piedmont Street	7	\$ 949,000.00					\$ 949,000.00	22
Woodrow Wilson to Riverside Drive	7	\$ 759,000.00					\$ 759,000.00	23
Bridges								24
Hanging Moss Road	2	\$ 633,000.00					\$ 633,000.00	26
Country Club Drive	4	\$ 345,000.00					\$ 345,000.00	27
Robinson Road	4,5	\$ 46,000.00					\$ 46,000.00	28
Alta Woods Boulevard	5	\$ 78,000.00					\$ 78,000.00	29
Ready Mix Street 2	7	\$ 460,000.00					\$ 460,000.00	30
Streets/Roads								31
County Line Road A-2	1	\$ 2,788,000.00					\$ 2,788,000.00	33
Medgar Evers Boulevard	3	\$ 7,000,000.00					\$ 7,000,000.00	34
State Street	3,7	\$ 3,452,000.00					\$ 3,452,000.00	35
Mill Street	3,7	\$ 1,812,000.00					\$ 1,812,000.00	36
John R. Lynch Street	5	\$ 1,135,000.00					\$ 1,135,000.00	37
Riverside Drive Phase 1	7	\$ 1,053,000.00	\$ 4,388,500.00	\$ 4,388,500.00			\$ 9,830,000.00	38
Riverside Drive Phase 2	7	\$ 548,000.00			\$ 2,282,500.00	\$ 2,282,500.00	\$ 5,113,000.00	39
Water Line Improvements								40
Eastover Drive	1	\$ 1,388,000.00					\$ 1,388,000.00	42
Lawrence Road	3	\$ 646,000.00					\$ 646,000.00	43
McLeod Street	5	\$ 268,000.00					\$ 268,000.00	44
Wooddell Drive Area	6	\$ 1,886,000.00					\$ 1,886,000.00	45
Program Management		\$ 2,500,000.00					\$ 2,500,000.00	47
Green Infrastructure		\$ 1,500,000.00					\$ 1,500,000.00	48
Flex Funds		\$ 5,200,000.00					\$ 5,200,000.00	49
Comprehensive Drainage Study		\$ 750,000.00					\$ 750,000.00	50
Grand Total		\$ 48,161,000.00	\$ 4,388,500.00	\$ 4,388,500.00	\$ 2,282,500.00	\$ 2,282,500.00	\$ 60,753,000.00	

Appendix	
Drainage Project Maps	DR-1 - DR-14
Bridge Project Maps	BR-1 - BR-5
Street Project Maps	ST-1 - ST-7
Water Line Improvement Maps	WT-1 - WT-4

City of Jackson, Mississippi

Capital Improvement Plan Projects

2015 thru 2019

DRAINAGE

<u>Project Name</u>	<u>Ward</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>	<u>Page</u>
Beechcrest Drive	1	\$1,044,000	-	-	-	-	\$1,044,000	10
North Canton Club	1	\$949,000	-	-	-	-	\$949,000	11
Woodhill Road to Beasley Road	2	\$1,848,000	-	-	-	-	\$1,848,000	12
George Washington Drive	2	\$1,848,000	-	-	-	-	\$1,848,000	13
Forest Avenue to Northside Drive	3	\$1,140,000	-	-	-	-	\$1,140,000	14
Woodrow Wilson to Erie Street	3	\$1,044,000	-	-	-	-	\$1,044,000	15
Parkway Dr. to Ave. D (Grove Park)	4	\$791,000	-	-	-	-	\$791,000	16
Brookwood Place	4	\$538,000	-	-	-	-	\$538,000	17
St. Charles Ave. to Robinson Road	5	\$1,232,000	-	-	-	-	\$1,232,000	18
Oakmont Drive to Primos Avenue	5	\$506,000	-	-	-	-	\$506,000	19
McDowell Road to Cooper Road	6	\$1,107,000	-	-	-	-	\$1,107,000	20
Lakeshore Road to Heritage Place	6	\$918,000	-	-	-	-	\$918,000	21
Belmont Street to Piedmont Street	7	\$949,000	-	-	-	-	\$949,000	22
Woodrow Wilson to Riverside Drive	7	\$759,000	-	-	-	-	\$759,000	23
TOTAL		\$14,673,000	-	-	-	-	\$14,673,000	



City of Jackson, Mississippi

Capital Improvement Plan Projects

2015 thru 2019

DRAINAGE

FUNDING SOURCES

<u>Source</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>
CIP Fund	-	-	-	-	-	-
Other Funds	-	-	-	-	-	-
MDOT Earmark	-	-	-	-	-	-
Other Sources To Be Determined	-	-	-	-	-	-
TOTAL	-	-	-	-	-	-

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Beechcrest Drive	PROJECT TYPE: Drainage Improvement	LOCATION: (ADDRESS OR CROSS STREET) Beechcrest Drive																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will improve the drainage system along Beechcrest Drive for an estimated linear footage of 1000 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$136,000 CONSTRUCTION \$ 908,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$1,044,000		REQUIRED FUNDING BY YEAR: <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>	CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$		OPERATING COST/BENEFIT: ANNUAL INCREASE/DECREASE PERSONNEL \$ SUPPLIES \$ OTHER \$ MAINTENANCE \$ TOTAL COST \$ ANNUAL REVENUES TOTAL \$
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: North Canton Club	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) North Canton Club																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along North Canton Club for an estimated linear footage of 1000 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Woodhill Rd. to Beasley Rd.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Woodhill Rd. to Beasley Rd.																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along Woodhill Road for an estimated linear footage of 2000 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: George Washington Dr.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) George Washington Dr.																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along George Washington Drive for an estimated linear footage of 2000 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Forest Avenue to Northside Dr.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Forest Avenue to Northside Dr.																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will improve the drainage system along Forest Avenue for an estimated linear footage of 1600 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Woodrow Wilson to Erie St.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Woodrow Wilson to Erie St.																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along Woodrow Wilson for an estimated linear footage of 1200 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																							
PROJECT NAME: Parkway Dr. to Avenue D (Grove Park)	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Parkway Dr. to Avenue D(Grove Park)																								
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along Parkway Drive for an estimated linear footage of 1000 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																							
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TOTAL	\$																									
DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																							

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Brookwood Place	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Brookwood Place																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will improve the drainage system along Brookwood Place for an estimated linear footage of 600 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
STATUS: SITE SELECTED <input checked="" type="checkbox"/> LAND ACQUIRED <input type="checkbox"/>		CONDEMNATION ORDERED <input type="checkbox"/> PRELIMINARY DESIGN <input type="checkbox"/>	PLANS/SPECIFICATIONS IN PROGRESS <input type="checkbox"/> PLANS/SPECIFICATIONS COMPLETED <input type="checkbox"/> CONSTRUCTION START DATE: COMPLETION DATE:																								
<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 70,000 CONSTRUCTION \$ 468,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 538,000		<u>REQUIRED FUNDING BY YEAR:</u> <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>	<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$		<u>OPERATING COST/BENEFIT:</u> <u>ANNUAL INCREASE/DECREASE</u> PERSONNEL \$ SUPPLIES \$ OTHER \$ MAINTENANCE \$ TOTAL COST \$ <u>ANNUAL REVENUES</u> TOTAL \$
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TOTAL \$	\$																										
DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																								

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: St. Charles Ave. to Robinson Rd.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) St. Charles Ave. to Robinson Rd.																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will improve the drainage system along St. Charles Avenue for an estimated linear footage of 1500 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$ 132,000 CONSTRUCTION \$ 1,100,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 1,232,000		REQUIRED FUNDING BY YEAR: <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>	CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$		OPERATING COST/BENEFIT: ANNUAL INCREASE/DECREASE PERSONNEL \$ SUPPLIES \$ OTHER \$ MAINTENANCE \$ TOTAL COST \$ ANNUAL REVENUES TOTAL \$
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DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																								

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Oakmont Dr. to Primos Ave	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Oakmont Dr. to Primos Ave.																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will improve the drainage system along Oakmont Drive for an estimated linear footage of 600 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 66,000 CONSTRUCTION \$ 440,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 506,000		<u>REQUIRED FUNDING BY YEAR:</u> <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																																																		
PROJECT NAME: McDowell Rd. to Cooper Rd.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) McDowell Rd. to Cooper Rd.																																																			
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along McDowell Road for an estimated linear footage of 1600 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																																																		
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<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 144,000 CONSTRUCTION \$ 963,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 1,107,000		<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th colspan="4"><u>REQUIRED FUNDING BY YEAR:</u></th> <th><u>OPERATING COST/BENEFIT:</u></th> </tr> <tr> <th><u>CITY FUNDS</u></th> <th><u>OTHER FUNDS</u></th> <th><u>SOURCES</u></th> <th></th> <th><u>ANNUAL INCREASE/DECREASE</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> <td></td> <td>PERSONNEL \$</td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> <td></td> <td>SUPPLIES \$</td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> <td></td> <td>OTHER \$</td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> <td></td> <td>MAINTENANCE \$</td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> <td></td> <td>TOTAL COST \$</td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> <td></td> <td></td> </tr> <tr> <td colspan="4">TOTAL \$</td> <td><u>ANNUAL REVENUES</u></td> </tr> <tr> <td colspan="4"></td> <td>TOTAL \$</td> </tr> </tbody> </table>		<u>REQUIRED FUNDING BY YEAR:</u>				<u>OPERATING COST/BENEFIT:</u>	<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>		<u>ANNUAL INCREASE/DECREASE</u>	FY 14-15 \$	\$			PERSONNEL \$	FY 15-16 \$	\$			SUPPLIES \$	FY 16-17 \$	\$			OTHER \$	FY 17-18 \$	\$			MAINTENANCE \$	FY 18-19 \$	\$			TOTAL COST \$	BEYOND-19 \$	\$				TOTAL \$				<u>ANNUAL REVENUES</u>					TOTAL \$
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Lakeshore Rd. to Heritage PL.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Lakeshore Rd. to Heritage Place																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along Lakeshore Road for an estimated linear footage of 800 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 120,000 CONSTRUCTION \$ 798,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 918,000		<u>REQUIRED FUNDING BY YEAR:</u> <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Belmont St. to Piedmont St.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Belmont St. to Piedmont St.																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along Belmont Street for an estimated linear footage of 600 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$ 1234,000 CONSTRUCTION \$ 825,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 949,000		REQUIRED FUNDING BY YEAR: <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>	<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$		OPERATING COST/BENEFIT: ANNUAL INCREASE/DECREASE PERSONNEL \$ SUPPLIES \$ OTHER \$ MAINTENANCE \$ TOTAL COST \$ <u>ANNUAL REVENUES</u> TOTAL \$
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Woodrow Wilson to Riverside Dr.	PROJECT TYPE: Drainage System	LOCATION: (ADDRESS OR CROSS STREET) Woodrow Wilson to Riverside Dr.																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will improve the drainage system along Woodrow Wilson for an estimated linear footage of 800 feet. Major activity includes clearing, grubbing, and bank stabilization to improve the drainage.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$ 99,000 CONSTRUCTION \$ 660,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 759,000		REQUIRED FUNDING BY YEAR: <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>	<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$		OPERATING COST/BENEFIT: <u>ANNUAL INCREASE/DECREASE</u> PERSONNEL \$ SUPPLIES \$ OTHER \$ MAINTENANCE \$ TOTAL COST \$ <u>ANNUAL REVENUES</u> TOTAL \$
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City of Jackson, Mississippi

Capital Improvement Plan Projects

2015 thru 2019

BRIDGES

<u>Project Name</u>	<u>Ward</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>	<u>Page</u>
Hanging Moss Road	2	\$633,000	-	-	-	-	\$633,000	26
Country Club Drive	4	\$345,000	-	-	-	-	\$345,000	27
Robinson Road	4,5	\$46,000	-	-	-	-	\$46,000	28
Alta Woods Boulevard	5	\$78,000	-	-	-	-	\$78,000	29
Ready Mix Street 2	7	\$460,000	-	-	-	-	\$460,000	30
TOTAL		\$1,562,000	-	-	-	-	\$1,562,000	



City of Jackson, Mississippi
Capital Improvement Plan Projects
2015 thru 2019
BRIDGES
FUNDING SOURCES

<u>Source</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>
CIP Fund	-	-	-	-	-	-
Other Funds	-	-	-	-	-	-
MDOT Earmark	-	-	-	-	-	-
Other Sources To Be Determined	-	-	-	-	-	-
TOTAL	-	-	-	-	-	-

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Hanging Moss Road	PROJECT TYPE: Bridge Repair	LOCATION: (ADDRESS OR CROSS STREET) Hanging Moss Road, tributary of Hanging Moss Creek																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will repair the Bridge, located on Hanging Moss Road, and the tributary of Hanging Moss Creek. Major activity includes the replacement of the bridge because of substandard load carrying capacity.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
STATUS: SITE SELECTED <input checked="" type="checkbox"/> CONDEMNATION ORDERED <input type="checkbox"/> PLANS/SPECIFICATIONS IN PROGRESS <input type="checkbox"/> LAND ACQUIRED <input type="checkbox"/> PRELIMINARY DESIGN <input type="checkbox"/> PLANS/SPECIFICATIONS COMPLETED <input type="checkbox"/>			CONSTRUCTION START DATE: COMPLETION DATE:																								
<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 83,000 CONSTRUCTION \$ 550,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 633,000		<u>REQUIRED FUNDING BY YEAR:</u> <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																								

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Country Club Drive	PROJECT TYPE: Bridge Repair	LOCATION: (ADDRESS OR CROSS STREET) Country Club Drive above Lynch Creek																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will repair the Bridge, located on Country Club Drive, above Lynch Creek. Major activity includes the replacement of the bridge because of substandard load carrying capacity.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 45,000 CONSTRUCTION \$ 300,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 345,000		<u>REQUIRED FUNDING BY YEAR:</u> <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Robinson Road	PROJECT TYPE: Bridge Repair	LOCATION: (ADDRESS OR CROSS STREET) Robinson Road, above Caney Creek																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will repair the Bridge, located on Robinson Road, above Caney Creek. Major activity includes the replacement of the bridge because of substandard load carrying capacity.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 6,000 CONSTRUCTION \$ 40,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 46,000		<u>REQUIRED FUNDING BY YEAR:</u> <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																								

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Alta Woods Boulevard	PROJECT TYPE: Bridge Repair	LOCATION: (ADDRESS OR CROSS STREET) Alta Woods Boulevard, above Small Stream																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will repair the Bridge, located on Alta Woods Boulevard, above Small Stream. Major activity includes the replacement of the bridge because of substandard load carrying capacity.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ 10,000 CONSTRUCTION \$ 68,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$78,000		<u>REQUIRED FUNDING BY YEAR:</u> <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																								

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE: 601-960-1651																								
PROJECT NAME: Ready Mix Street 2	PROJECT TYPE: Bridge Repair	LOCATION: (ADDRESS OR CROSS STREET) Hanging Moss Road, tributary to Town Creek																									
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will repair the Bridge, located on Ready Mix Street, and the tributary to Town Creek. Major activity includes the replacement of the bridge because of substandard load carrying capacity.			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) () Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure <hr/> FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$ 60,000 CONSTRUCTION \$ 400,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$ 460,000		REQUIRED FUNDING BY YEAR: <table border="1"> <thead> <tr> <th>CITY FUNDS</th> <th>OTHER FUNDS</th> <th>SOURCES</th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		CITY FUNDS	OTHER FUNDS	SOURCES	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																								

City of Jackson, Mississippi

Capital Improvement Plan Projects

2015 thru 2019

STREETS

<u>Project Name</u>	<u>Ward</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>	<u>Page</u>
County Line Road A-2	1	\$2,788,000	-	-	-	-	\$2,788,000	33
Medgar Evers Boulevard	3	\$7,000,000	-	-	-	-	\$7,000,000	34
State Street	3,7	\$3,245,000	-	-	-	-	\$3,245,000	35
Mill Street	3,7	\$1,812,000	-	-	-	-	\$1,812,000	36
John R. Lynch Street	5	\$1,135,000	-	-	-	-	\$1,135,000	37
Riverside Drive Phase 1	7	\$1,053,000	\$4,388,500	\$4,388,500	-	-	\$9,830,000	38
Riverside Drive Phase 2	7	\$548,000	-	-	\$2,282,500	\$2,282,500	\$5,113,000	39
TOTAL		\$17,788,000	\$4,388,500	\$4,388,500	\$2,282,500	\$2,282,500	\$31,130,000	



City of Jackson, Mississippi

Capital Improvement Plan Projects

2015 thru 2019

STREETS

FUNDING SOURCES

<u>Source</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>
CIP Fund	-	-	-	-	-	-
Other Funds	-	-	-	-	-	-
MDOT Earmark	-	-	-	-	-	-
Other Sources To Be Determined	-	-	-	-	-	-
TOTAL	-	-	-	-	-	-

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Robert Lee	PHONE: 601-960-1651																								
PROJECT NAME: County Line Road A-2	PROJECT TYPE: Roadway Resurfacing, Signal Repair, Sidewalk Installation	LOCATION: (ADDRESS OR CROSS STREET) This stretch will run from Ridgewood Road, to Pear Orchard Road, along County Line Road.																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will repair County Line Road between Pear Orchard Road and 400' west of Ridgewood Road. Major activity includes milling and overlay of the existing 4 lane road with new asphalt, sidewalk installation, and the repair of traffic signals.</p> <p>(Note: Due to traffic restrictions, most of the construction will take place at night.)</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>(X) Needed for the Protection of Life and/or Property</p> <p>() Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY:</p> <p>() Included in Comprehensive Plan</p> <p>_____ Funded</p> <p>_____ Unfunded</p>																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$299,000 CONSTRUCTION \$2,489,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$2,788,000		REQUIRED FUNDING BY YEAR: <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>	<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$		OPERATING COST/BENEFIT: ANNUAL INCREASE/DECREASE PERSONNEL \$ SUPPLIES \$ OTHER \$ MAINTENANCE \$ TOTAL COST \$ <u>ANNUAL REVENUES</u> TOTAL \$
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Robert Lee	PHONE: 601-960-1651																								
PROJECT NAME: Medgar Evers Boulevard	PROJECT TYPE: Roadway Reconstruction	LOCATION: (ADDRESS OR CROSS STREET) This stretch will run from Sunset Drive, to Northside Drive along Medgar Evers Boulevard.																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will completely reconstruct Medgar Evers Boulevard between Sunset Drive and Northside Drive. Major activity includes the rebuilding/replacing of the existing 4 lane boulevard with new asphalt, traffic signal replacement, sewer line rehabilitation, sidewalk installation, and drainage improvement.</p> <p>(Note: As part of this project, a Water and Sewer study will be developed on Medgar Evers Boulevard. Also a Traffic Impact Study will be performed.)</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>() Needed for the Protection of Life and/or Property</p> <p>(X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY:</p> <p>() Included in Comprehensive Plan</p> <p>_____ Funded</p> <p>_____ Unfunded</p>																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$750,000 CONSTRUCTION \$6,250,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$7,000,000		REQUIRED FUNDING BY YEAR: <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Robert Lee	PHONE: 601-960-1651																																													
PROJECT NAME: State Street	PROJECT TYPE: Roadway Resurfacing, Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) This stretch will run from Seneca Avenue to Sheppard Road, along State Street.																																														
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will repair State Street between Seneca Avenue and Sheppard Road. Major activity includes the surface coating of the existing 3 lane road with new asphalt, sewer repair, the repair of traffic signals, and:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th style="width: 25%;">Street</th> <th style="width: 45%;">Project Description</th> <th style="width: 30%;">Estimated Linear Footage</th> </tr> <tr> <td rowspan="3" style="text-align: center; vertical-align: middle;">State Street</td> <td>Upgrade 6" Line to 8" Line</td> <td style="text-align: center;">1250</td> </tr> <tr> <td>Repair 12" Line</td> <td style="text-align: center;">175</td> </tr> <tr> <td>Repair 24" Line</td> <td style="text-align: center;">265</td> </tr> </table>			Street	Project Description	Estimated Linear Footage	State Street	Upgrade 6" Line to 8" Line	1250	Repair 12" Line	175	Repair 24" Line	265	<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project () Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																																			
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Robert Lee	PHONE: 601-960-1651																																																															
PROJECT NAME: Mill Street	PROJECT TYPE: Roadway Reconstruction, Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) This stretch will run from Mitchell Avenue to Woodrow Wilson Avenue, along Mill Street.																																																																
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will repair Mill Street between Mitchell Avenue and Woodrow Wilson Avenue. Major activity includes the rebuilding/replacing of the existing 2 lane road with new asphalt, sewer repair, drainage improvement, and:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <tr> <th style="width: 25%;">Street</th> <th style="width: 45%;">Project Description</th> <th style="width: 30%;">Estimated Linear Footage</th> </tr> <tr> <td rowspan="2" style="text-align: center; vertical-align: middle;">Mill Street</td> <td>Upgrade 8" Line to 10" Line</td> <td style="text-align: center;">620</td> </tr> <tr> <td>Repair 10" Line</td> <td style="text-align: center;">2590</td> </tr> </table>			Street	Project Description	Estimated Linear Footage	Mill Street	Upgrade 8" Line to 10" Line	620	Repair 10" Line	2590	<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property () Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																																																							
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Robert Lee	PHONE: 601-960-1651																																													
PROJECT NAME: John R. Lynch Street	PROJECT TYPE: Roadway Resurfacing, Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) This stretch will run from Highway 80 to Yarbro Street, along John R. Lynch Street.																																														
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will repair John R. Lynch Street between Highway 80 East and Yarbro Street. Major activity includes milling of the existing pavement, replacement of the pavement with new asphalt, repair of the traffic signal at the intersection of John R. Lynch Street and West Highland Drive, fire protection, relocating of the existing sewer line, drainage improvement, and:</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>(X) Needed for the Protection of Life and/or Property</p> <p>() Needed to correct hazardous conditions in public infrastructure</p>																																													
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Robert Lee	PHONE: 601-960-1651																																																															
PROJECT NAME: Riverside Drive Phase 1	PROJECT TYPE: Roadway Reconstruction, Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) The stretch will run from Peachtree Street to the Bridge (crossing of I-55), along Riverside Drive.																																																																
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will repair Riverside Drive between Peachtree Street and I-55 Bridge. Major activity includes the rebuilding/replacing of the existing 4 lane road with new asphalt of a 3 lane road, with a shared path, fire protection and drainage improvement. Major activity also includes:</p> <ul style="list-style-type: none"> Riverside Drive 1 involves the replacement of the 16" water line, running from the I-55 Bridge to Peachtree Street. Riverside Drive 2 involves the replacement of the 36" water line, running from the I-55 Bridge to Peachtree Street. <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <tr> <th style="width: 25%;">Street</th> <th style="width: 45%;">Project Description</th> <th style="width: 30%;">Estimated Linear Footage</th> </tr> <tr> <td>Riverside Drive 1</td> <td>Replace 16" Line</td> <td>3700</td> </tr> <tr> <td>Riverside Drive 2</td> <td>Replace 36" Line</td> <td>2750</td> </tr> </table>			Street	Project Description	Estimated Linear Footage	Riverside Drive 1	Replace 16" Line	3700	Riverside Drive 2	Replace 36" Line	2750	<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>(X) Needed for the Protection of Life and/or Property</p> <p>() Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY:</p> <p>() Included in Comprehensive Plan</p> <p>_____ Funded</p> <p>_____ Unfunded</p>																																																						
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Robert Lee	PHONE: 601-960-1651																																																																								
PROJECT NAME: Riverside Drive Phase 2	PROJECT TYPE: Roadway Reconstruction, Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) The stretch will run from State Street to Peachtree Street, along Riverside Drive.																																																																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will repair Riverside Drive between State Street and Peachtree Street. Major activity includes the rebuilding/replacing of the existing 4 lane road with new asphalt of a 3 lane road, with a shared path, fire protection, and drainage improvement. . Major activity also includes:</p> <ul style="list-style-type: none"> Riverside Drive 3 involves the replacement of the 16" water line, running from Peachtree Street to State Street. <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <tr> <td style="width: 30%;">Street</td> <td style="width: 40%;">Project Description</td> <td style="width: 30%;">Estimated Linear Footage</td> </tr> <tr> <td>Riverside Drive 3</td> <td>Replace 16" Line</td> <td>1800</td> </tr> </table>			Street	Project Description	Estimated Linear Footage	Riverside Drive 3	Replace 16" Line	1800	<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>(X) Needed for the Protection of Life and/or Property</p> <p>() Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY:</p> <p>() Included in Comprehensive Plan</p> <p>_____ Funded</p> <p>_____ Unfunded</p>																																																																		
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City of Jackson, Mississippi
Capital Improvement Plan Projects
2015 thru 2019

WATER LINE IMPROVEMENTS

<u>Project Name</u>	<u>Ward</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>	<u>Page</u>
Eastover Drive	1	\$1,388,000	-	-	-	-	\$1,388,000	51
Lawrence Road	3	\$646,000	-	-	-	-	\$646,000	52
McLeod Street	5	\$268,000	-	-	-	-	\$268,000	53
Wooddell Drive Area	6	\$1,886,000	-	-	-	-	\$1,886,000	54
TOTAL		\$4,188,000	-	-	-	-	\$4,188,000	



City of Jackson, Mississippi
Capital Improvement Plan Projects
2015 thru 2019
WATER LINE IMPROVEMENTS
FUNDING SOURCES

<u>Source</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019</u>	<u>Total</u>
CIP Fund	-	-	-	-	-	-
Other Funds	-	-	-	-	-	-
MDOT Earmark	-	-	-	-	-	-
Other Sources To Be Determined	-	-	-	-	-	-
TOTAL	-	-	-	-	-	-

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Water and Wastewater	CONTACT PERSON: Bill Miley	PHONE: 601-960-2090																																													
PROJECT NAME: Eastover Drive	PROJECT TYPE: Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) Ward 1																																														
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will replace water lines associated in Ward 1.</p> <ul style="list-style-type: none"> • Eastover Drive 1 runs from I-55 North to Ridgewood Road. • Eastover Drive 2 runs from Twin Lakes Circle to Dogwood Drive. <p>Fire protection will be included in these projects. Major activity includes milling and overlay of the road, also:</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>(X) Needed for the Protection of Life and/or Property</p> <p>(X) Needed to correct hazardous conditions in public infrastructure</p>																																													
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Water and Wastewater	CONTACT PERSON: Bill Miley	PHONE: 601-960-2090																																																																									
PROJECT NAME: Lawrence Road	PROJECT TYPE: Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) This stretch will run from Durrey Street to Manhattan Road, along Lawrence Road.																																																																										
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will replace water lines associated in Ward 3. Fire protection, mill and overlay, will be included in this project. Major activity includes milling and overlay of the road, also:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <tr> <th style="width: 25%;">Street</th> <th style="width: 45%;">Project Description</th> <th style="width: 30%;">Estimated Linear Footage</th> </tr> <tr> <td>Lawrence Road</td> <td>Replace 6" Line</td> <td>3400</td> </tr> </table>			Street	Project Description	Estimated Linear Footage	Lawrence Road	Replace 6" Line	3400	<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																																																																			
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Water and Wastewater	CONTACT PERSON: Bill Miley	PHONE: 601-960-2090																																																												
PROJECT NAME: McLeod Street	PROJECT TYPE: Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) The stretch will run from Bailey Avenue to Monument Street, along McLeod Street.																																																													
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The proposed project will replace water lines associated in Ward 5. Fire protection will be included in this project. Major activity includes milling and overlay of the road, also:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <tr> <th style="width: 25%;">Street</th> <th style="width: 45%;">Project Description</th> <th style="width: 30%;">Estimated Linear Footage</th> </tr> <tr> <td>McLeod Street</td> <td>Upgrade 4" Line to 6" Line</td> <td>1250</td> </tr> </table>			Street	Project Description	Estimated Linear Footage	McLeod Street	Upgrade 4" Line to 6" Line	1250	<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project (X) Needed for the Protection of Life and/or Property (X) Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																																																						
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION: Water and Wastewater	CONTACT PERSON: Bill Miley	PHONE: 601-960-2090																																																															
PROJECT NAME: Wooddell Drive Area	PROJECT TYPE: Water Line Replacement	LOCATION: (ADDRESS OR CROSS STREET) Ward 6																																																																
PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS) The proposed project will replace water lines associated in Ward 6. <ul style="list-style-type: none"> • Wooddell Drive runs from McDowell Road to Oak Forest Drive. • Cliffwood Drive runs from Wooddell Drive to McDowell Road. • Lakewood Road runs from Wooddell Drive to McDowell Road. • Oak Forest Drive runs from Smallwood Street to McDowell Road. Fire protection will be included in these projects. Major activity includes milling and overlaying of each road, also:			PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.) <input type="checkbox"/> Funded Project (Partially Funded) <input type="checkbox"/> Mandated Project <input checked="" type="checkbox"/> Needed for the Protection of Life and/or Property <input checked="" type="checkbox"/> Needed to correct hazardous conditions in public infrastructure																																																															
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION:	CONTACT PERSON: Kishia Powell	PHONE:																								
PROJECT NAME:	PROJECT TYPE: Program Management	LOCATION: (ADDRESS OR CROSS STREET) City Wide																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The Program Management team will focus on achieving the desired outcomes of the Bold New Infrastructure Improvement Program. The Program Manager will be responsible for technical oversight, QA/QC, project delivery, and urgent need design. They will also oversee GI Challenge implementation and development of the City's Green Infrastructure strategy.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project () Needed for the Protection of Life and/or Property () Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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<u>ESTIMATED COST:</u> Land \$ ENGINEERING/DESIGN \$ CONSTRUCTION \$2,500,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$2,500,000		<u>REQUIRED FUNDING BY YEAR:</u> <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>	<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$		<u>OPERATING COST/BENEFIT:</u> <u>ANNUAL INCREASE/DECREASE</u> PERSONNEL \$ SUPPLIES \$ OTHER \$ MAINTENANCE \$ TOTAL COST \$ <u>ANNUAL REVENUES</u> TOTAL \$
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DEPARTMENTAL PROJECT NO.		CIP PROJECT NO.	TOTAL SCORE																								

THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

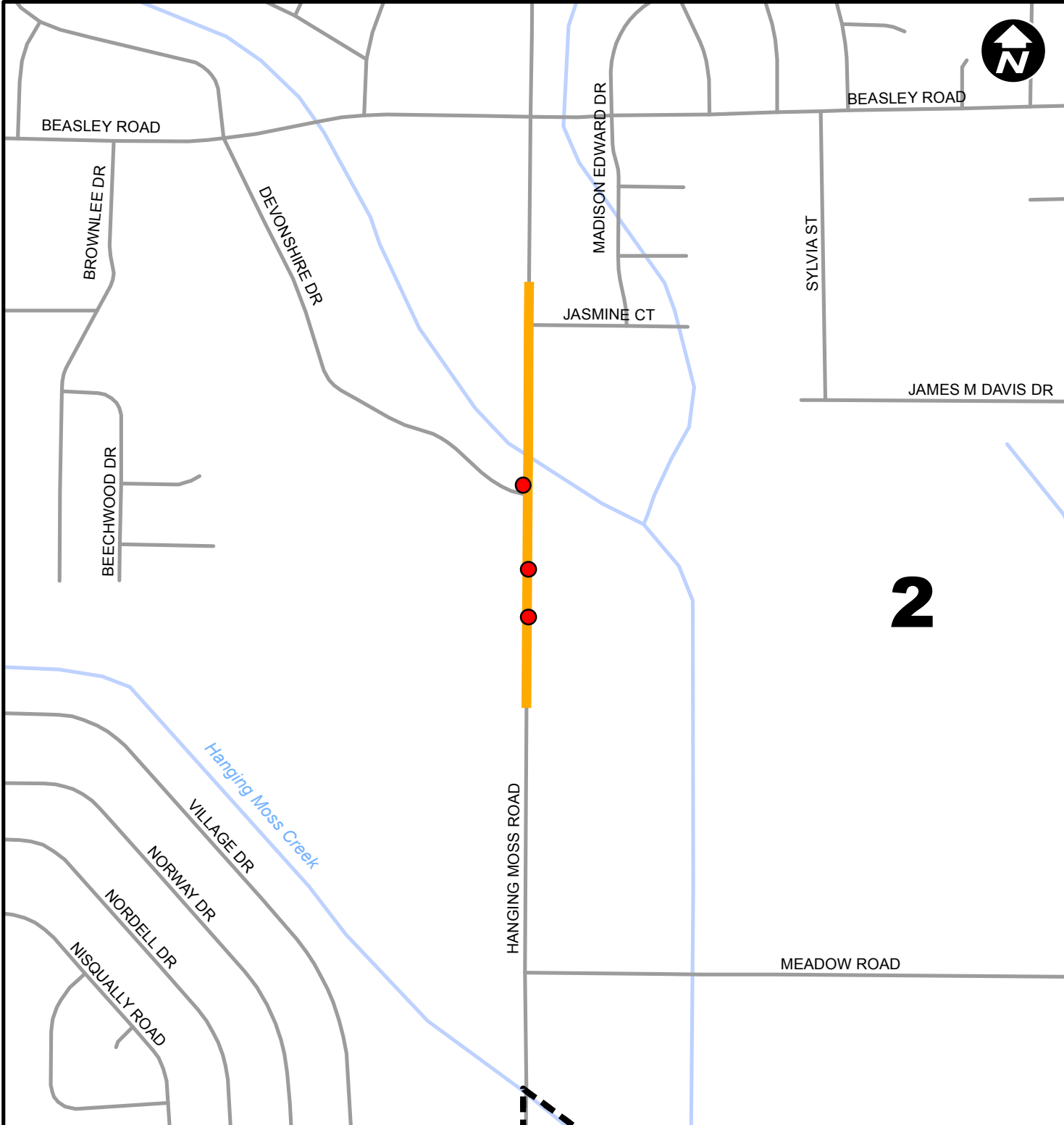
DEPARTMENT: Public Works	DIVISION:	CONTACT PERSON: Kishia Powell	PHONE:																								
PROJECT NAME: Green Infrastructure	PROJECT TYPE: Green Infrastructure	LOCATION: (ADDRESS OR CROSS STREET) City Wide																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The purpose of this project is to use a Green Infrastructure Challenge as part of the implementation strategy for Year 1 projects. This will allow the city to test the feasibility of these practices and assess cost for implementation maintenance.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>() Needed for the Protection of Life and/or Property</p> <p>() Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY:</p> <p>() Included in Comprehensive Plan</p> <p>_____ Funded</p> <p>_____ Unfunded</p>																								
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

DEPARTMENT: Public Works	DIVISION:	CONTACT PERSON: Kishia Powell	PHONE:																								
PROJECT NAME: Flex Funds	PROJECT TYPE: Water, Sewer, and Roads	LOCATION: (ADDRESS OR CROSS STREET) City Wide																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The Flex Funds will be used to improve City-Wide Infrastructure. These funds will be setup to be used at the discretion of the city for equipment purchase for the storm drain system asset management, and for repair/replacement of the city's on-call/urgent need construction.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded)</p> <p>() Mandated Project</p> <p>() Needed for the Protection of Life and/or Property</p> <p>() Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY:</p> <p>() Included in Comprehensive Plan</p> <p>_____ Funded</p> <p>_____ Unfunded</p>																								
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ESTIMATED COST: Land \$ ENGINEERING/DESIGN \$ CONSTRUCTION \$5,200,000 EQUIPMENT \$ OTHER \$ TOTAL COST: \$5,200,000		REQUIRED FUNDING BY YEAR: <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>CITY FUNDS</u></th> <th style="text-align: left;"><u>OTHER FUNDS</u></th> <th style="text-align: left;"><u>SOURCES</u></th> </tr> </thead> <tbody> <tr> <td>FY 14-15 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 15-16 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 16-17 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 17-18 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>FY 18-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>BEYOND-19 \$</td> <td>\$</td> <td></td> </tr> <tr> <td>TOTAL \$</td> <td>\$</td> <td></td> </tr> </tbody> </table>		<u>CITY FUNDS</u>	<u>OTHER FUNDS</u>	<u>SOURCES</u>	FY 14-15 \$	\$		FY 15-16 \$	\$		FY 16-17 \$	\$		FY 17-18 \$	\$		FY 18-19 \$	\$		BEYOND-19 \$	\$		TOTAL \$	\$	
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THE CITY OF JACKSON CAPITAL IMPROVEMENT PLAN – INDIVIDUAL PROJECT REPORT

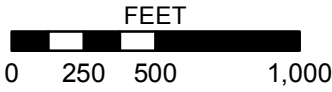
DEPARTMENT: Public Works	DIVISION: Engineering	CONTACT PERSON: Charles Williams	PHONE:																								
PROJECT NAME: Comprehensive Drainage Study	PROJECT TYPE: Drainage Improvement	LOCATION: (ADDRESS OR CROSS STREET) City Wide																									
<p>PROJECT DESCRIPTION: (BRIEF NARRATIVE DESCRIBING OVERALL PURPOSE, MAJOR ACTIVITIES, UNIQUE FEATURES, AND CURRENT STATUS)</p> <p>The purpose of this project is to develop a Comprehensive Drainage Study for the City of Jackson. This drainage study will examine the major water ways and their tributaries. A Hydraulic Model will be developed for each major watershed that will be used to examine the current capacities, and to identify future projects to alleviate flooding in low lying area.</p>			<p>PLEASE INDICATE WHICH OF THE FOLLOWING CRITERIA APPLIES TO THIS PROJECT. (Please check all that are relevant.)</p> <p>() Funded Project (Partially Funded) () Mandated Project () Needed for the Protection of Life and/or Property () Needed to correct hazardous conditions in public infrastructure</p> <hr/> <p>FOR CIP COMMITTEE ONLY: () Included in Comprehensive Plan _____ Funded _____ Unfunded</p>																								
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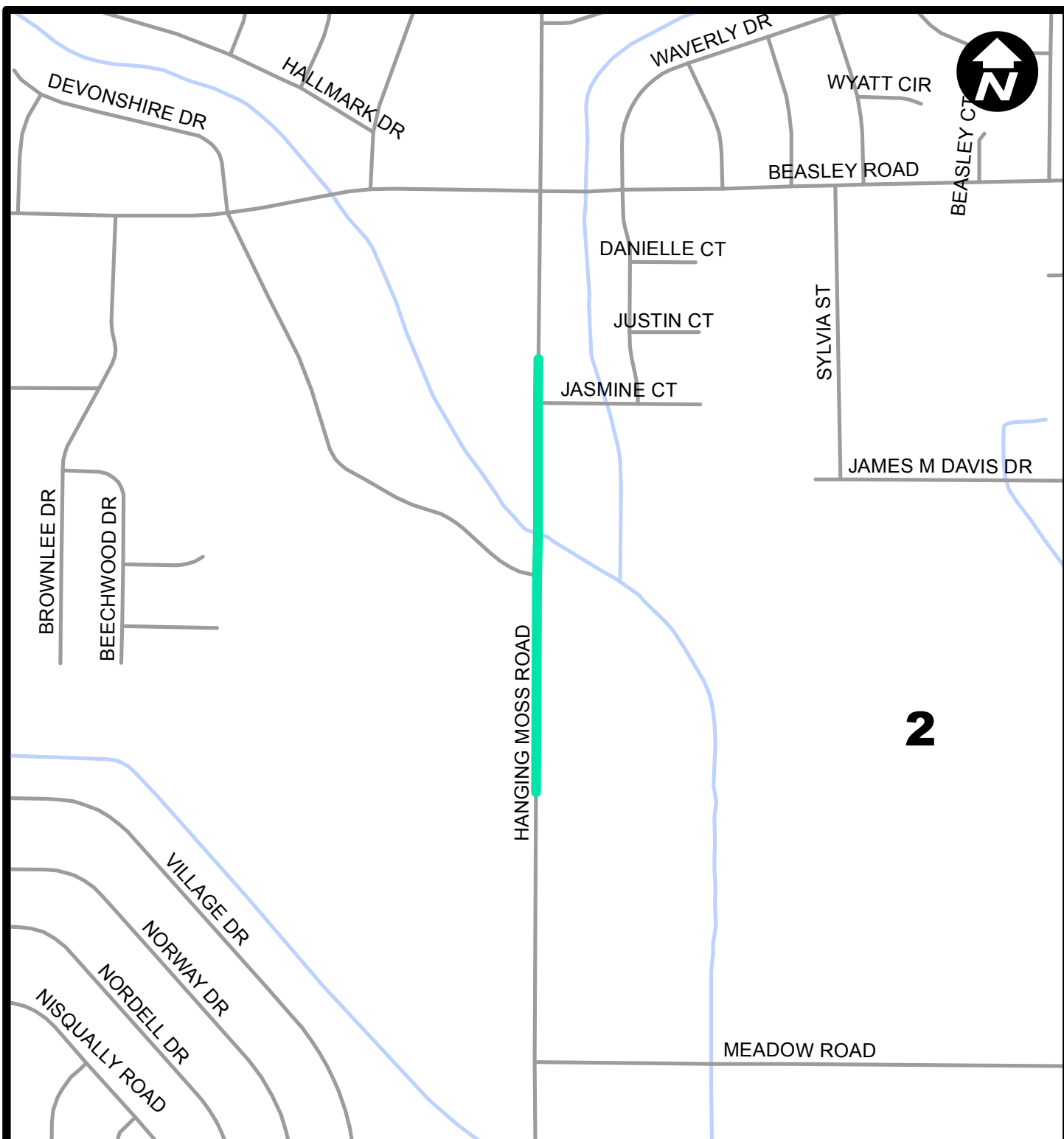


CITY OF JACKSON
MISSISSIPPI
CAPITAL IMPROVEMENT PLAN
BRIDGES

- LEGEND
- BRIDGE PROJECT
 - STREAMS
 - WATERBODIES
 - ROADS
 - 4 JACKSON WARD NUMBER
 - JACKSON WARD BOUNDARY
 - POTHOLE COMPLAINTS (3)

PROJECT NAME:
HANGING MOSS ROAD





CITY OF
JACKSON
MISSISSIPPI

CAPITAL IMPROVEMENT PLAN
BRIDGES

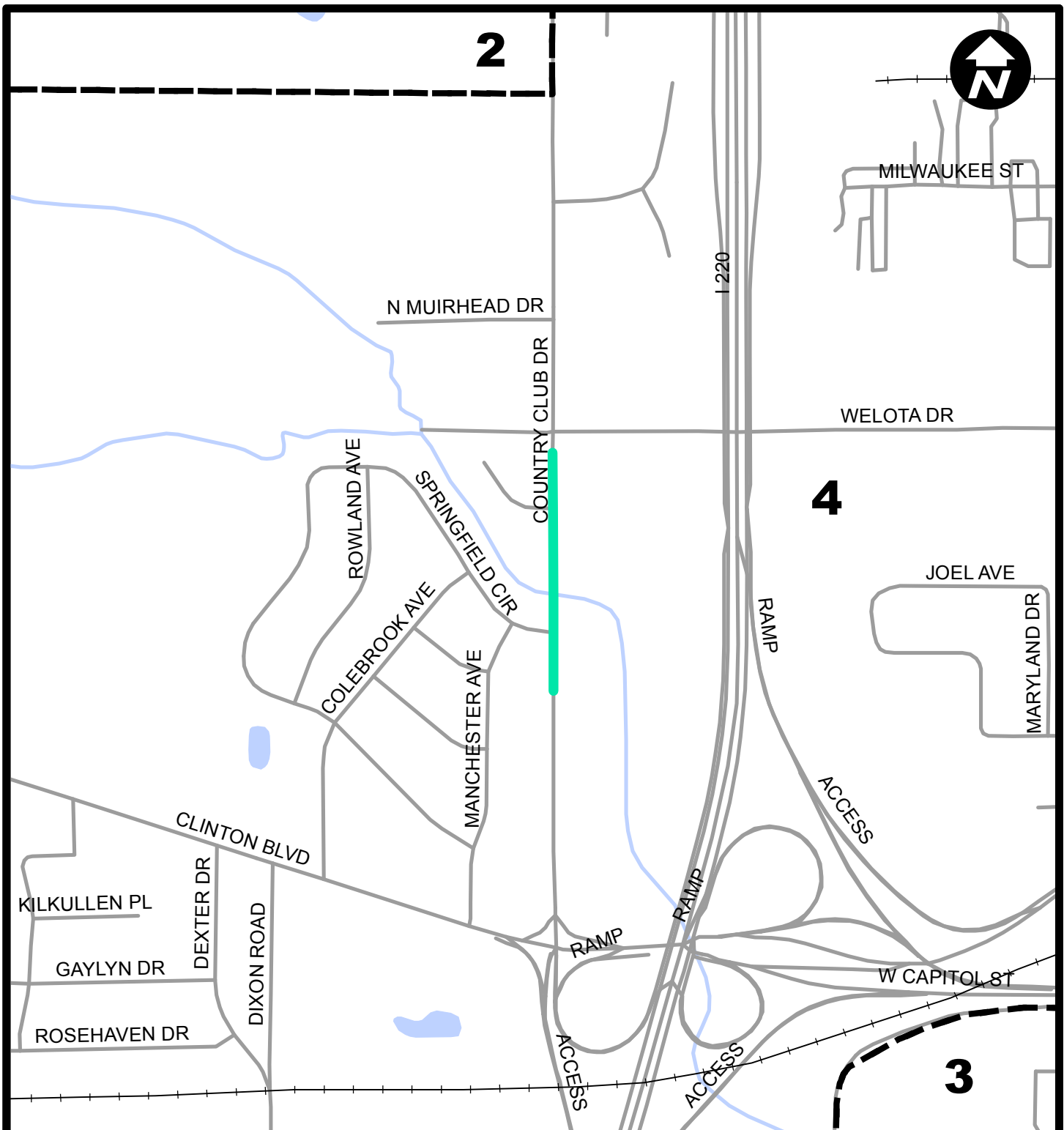
FIGURE BR-01

LEGEND

- BRIDGE PROJECT
- BRIDGE MAINTENANCE SERVICE REQUEST
- STREAMS
- WATERBODIES
- ROADS
- 2** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY

PROJECT NAME:
HANGING MOSS ROAD

0 250 500 1,000
FEET



CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN BRIDGES

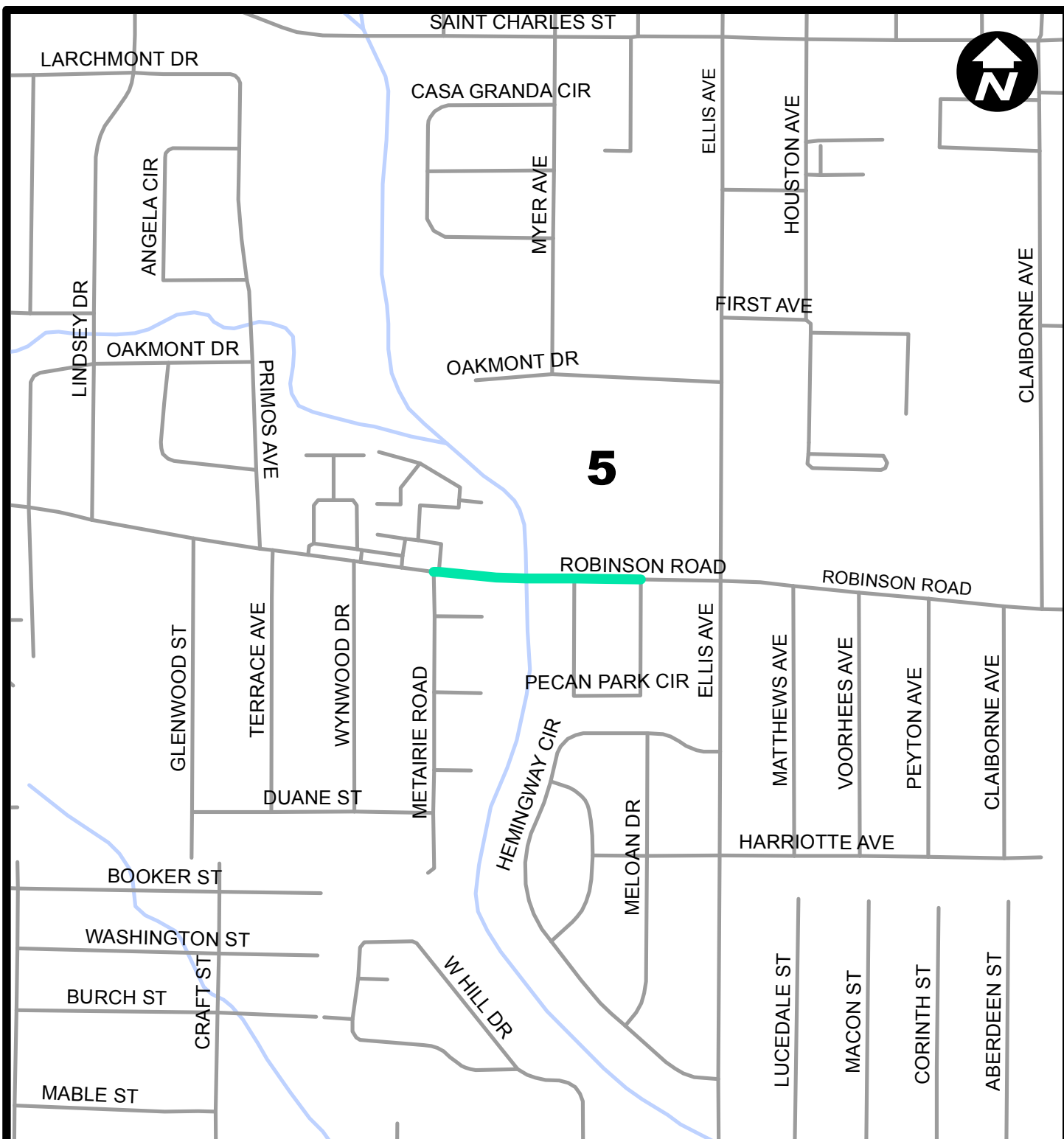
FIGURE BR-02

LEGEND

- BRIDGE PROJECT
- BRIDGE MAINTENANCE SERVICE REQUEST
- STREAMS
- WATERBODIES
- ROADS
- 2** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY

PROJECT NAME:
COUNTRY CLUB DRIVE

0 250 500 1,000
FEET



CITY OF
JACKSON
MISSISSIPPI

CAPITAL IMPROVEMENT PLAN
BRIDGES

FIGURE BR-03

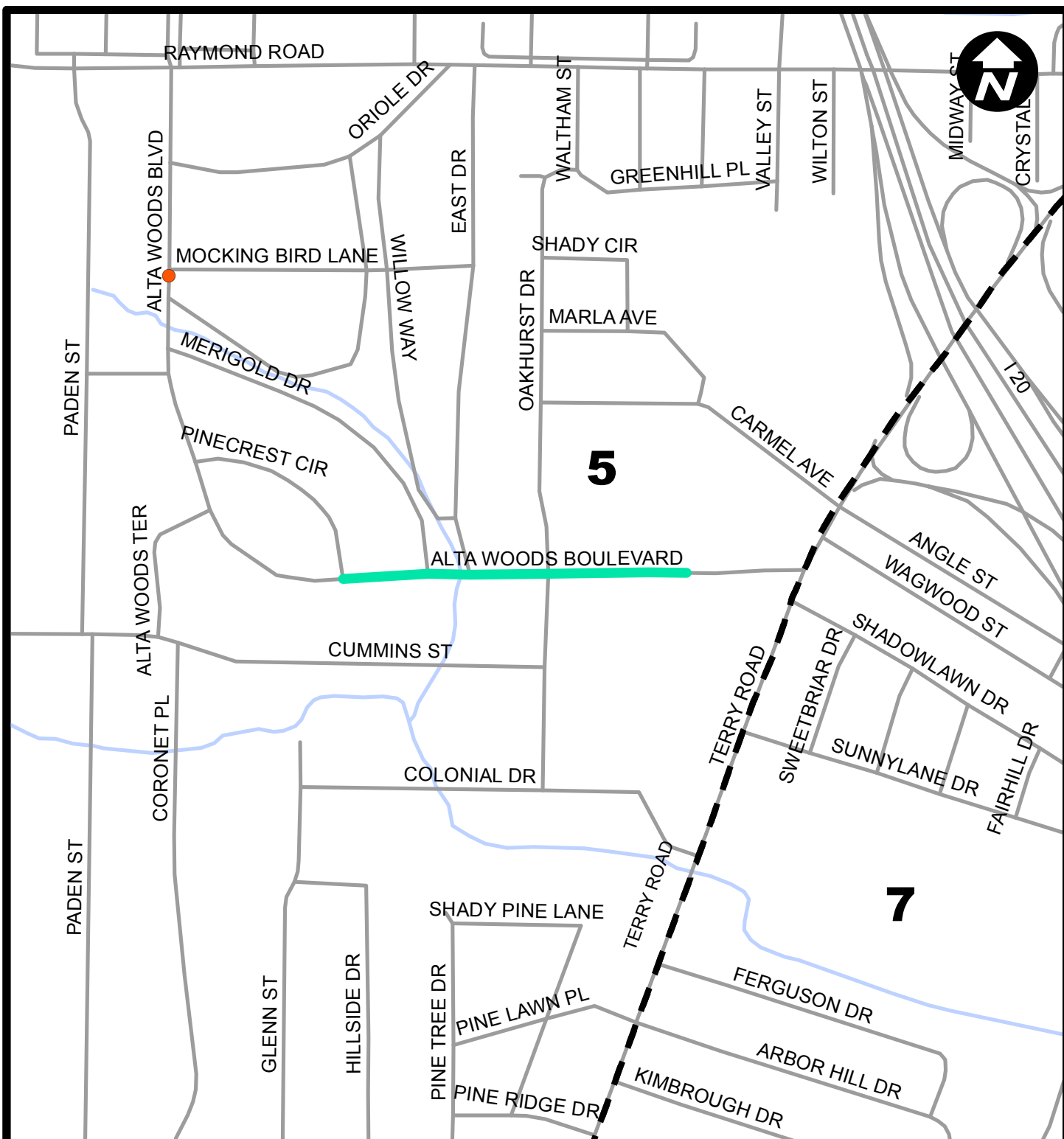
LEGEND

- BRIDGE PROJECT
- BRIDGE MAINTENANCE SERVICE REQUEST
- STREAMS
- WATERBODIES
- ROADS
- 5** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY

PROJECT NAME:
ROBINSON ROAD

0 250 500 1,000
FEET

FEBRUARY 2015



CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN BRIDGES

FIGURE BR-04

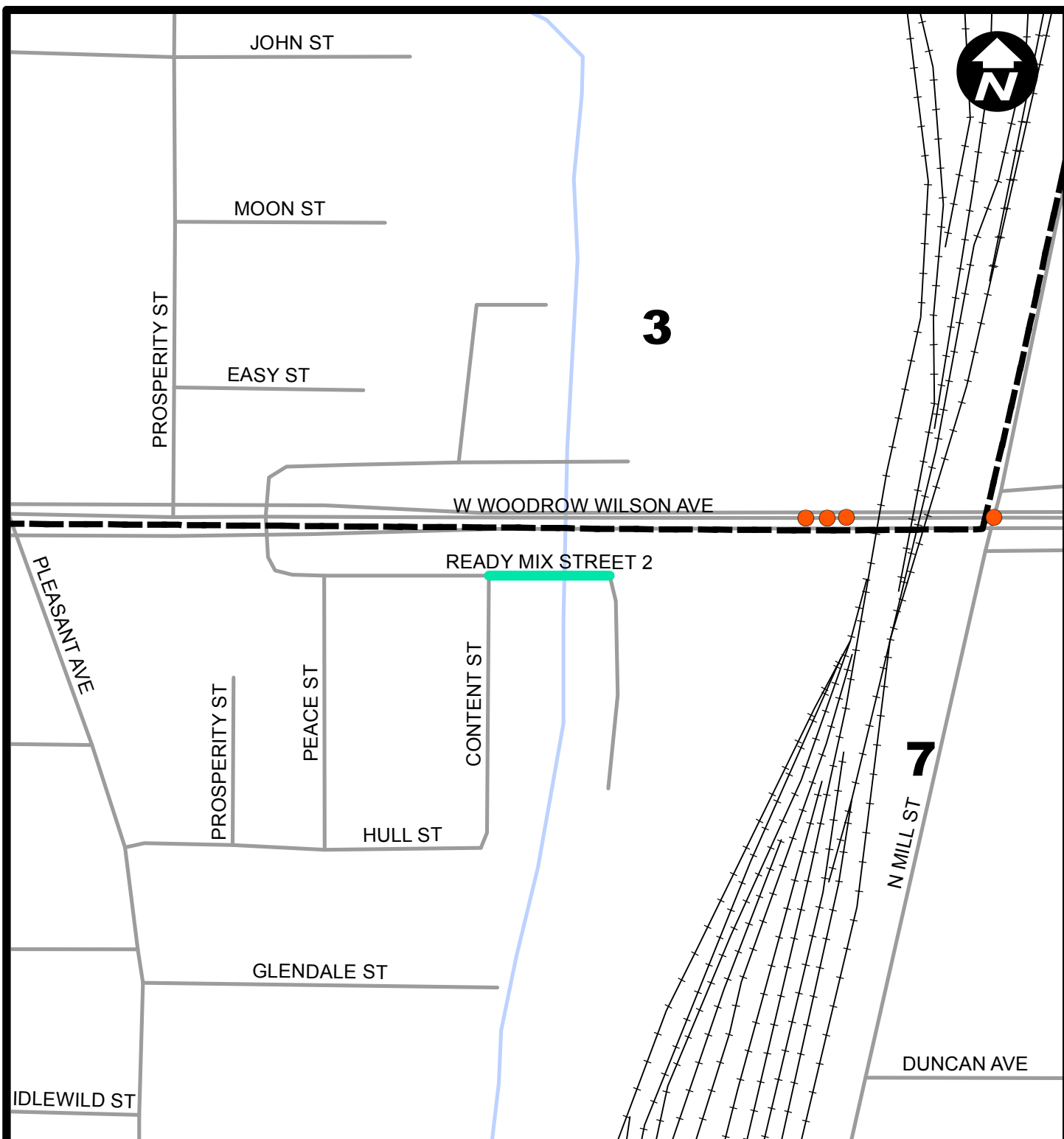
LEGEND

- █ BRIDGE PROJECT
- BRIDGE MAINTENANCE SERVICE REQUEST
- STREAMS
- WATERBODIES
- ROADS
- 5** JACKSON WARD NUMBER
- - - JACKSON WARD BOUNDARY

PROJECT NAME:
ALTA WOODS BLVD

0 250 500 1,000
FEET

FEBRUARY 2015



CITY OF
JACKSON
MISSISSIPPI

CAPITAL IMPROVEMENT PLAN
BRIDGES

FIGURE BR-05

LEGEND

- █ BRIDGE PROJECT
- BRIDGE MAINTENANCE SERVICE REQUEST
- STREAMS
- WATERBODIES
- ROADS
- 5** JACKSON WARD NUMBER
- - - JACKSON WARD BOUNDARY

PROJECT NAME:
READY MIX STREET 2

0 75 150 300
FEET

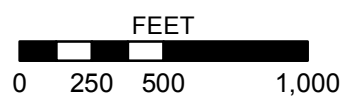
FEBRUARY 2015

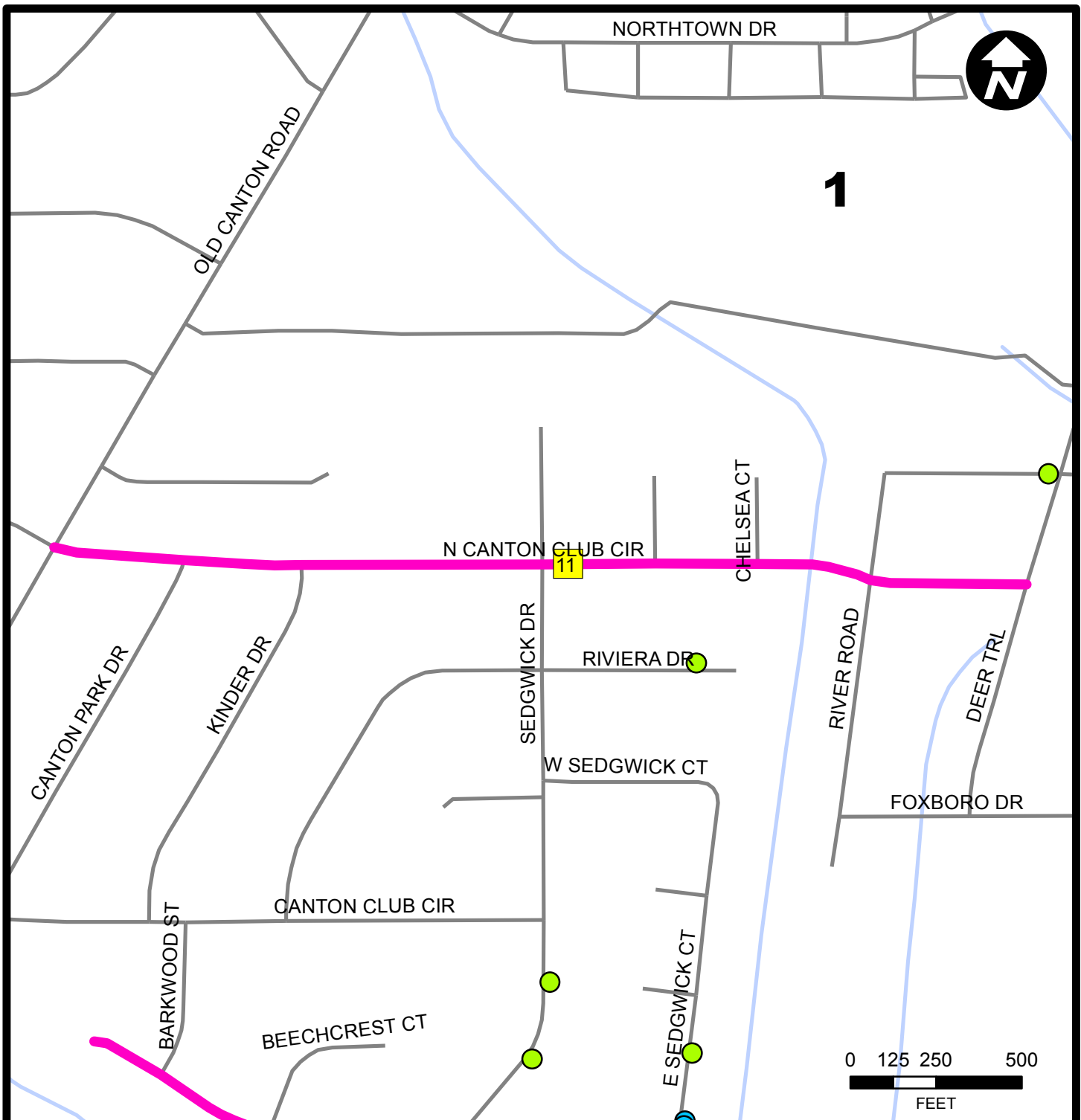


CITY OF JACKSON
MISSISSIPPI
CAPITAL IMPROVEMENT PLAN
DRAINAGE

- LEGEND**
- DRAINAGE PROJECT
 - STREAMS
 - WATERBODIES
 - ROADS
 - 4** JACKSON WARD NUMBER
 - JACKSON WARD BOUNDARY
 - DRAINAGE COMPLAINTS (3)

PROJECT NAME:
GEORGE WASHINGTON DRIVE





CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN DRAINAGE

PROJECT NAME:
N CANTON CLUB

FIGURE DR-01

LEGEND

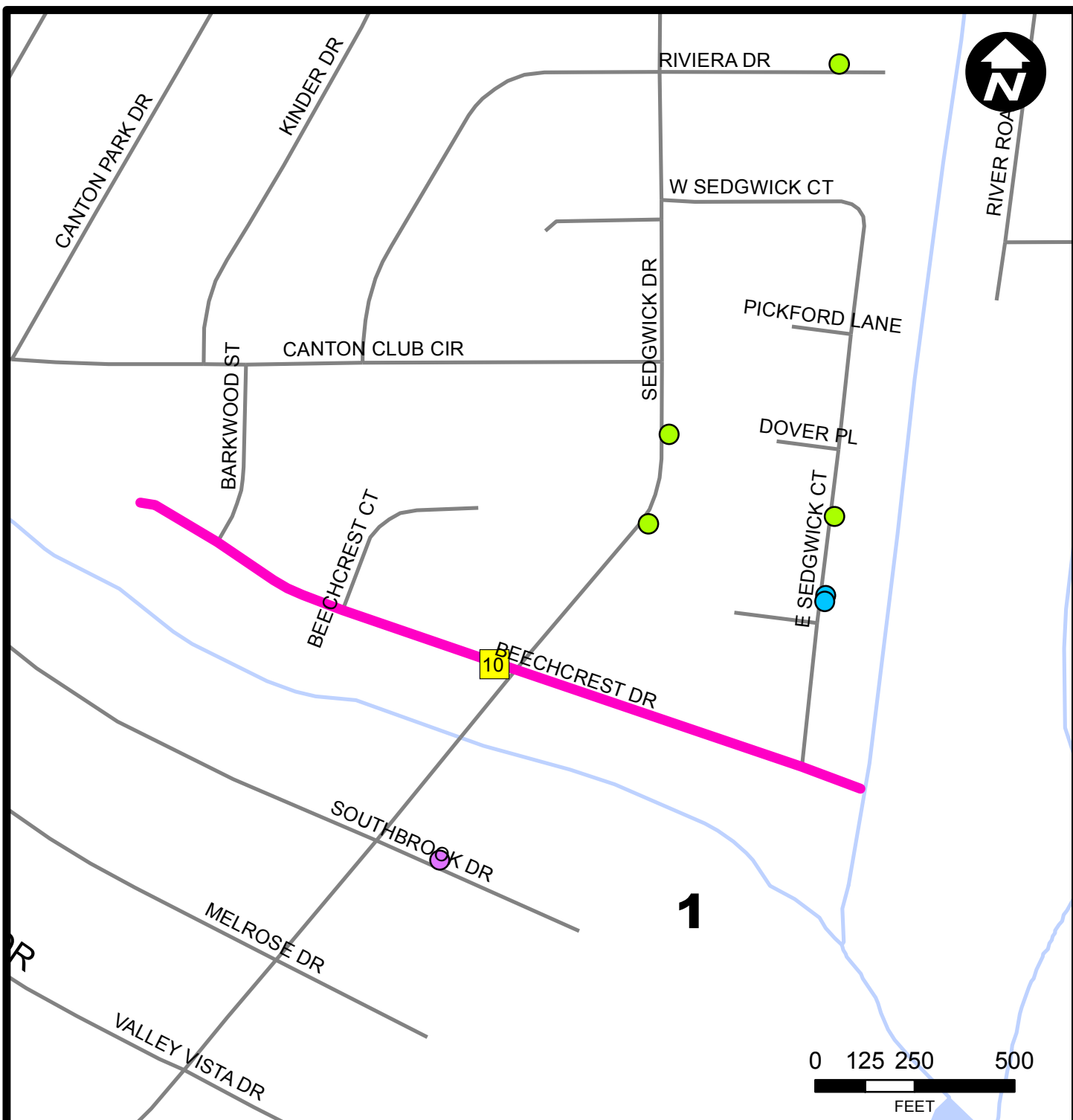
- DRAINAGE PROJECT
- STREAMS
- WATERBODIES
- ROADS
- 1** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY

2013 RECOMMENDED DRAINAGE PROJECTS

11 N CANTON CLUB

DRAINAGE COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- COMPLAINT REGARDING DRAINAGE PROJECT
- CURB/GUTTER MAINTENANCE
- DITCH MAINTENANCE
- DRAINAGE MAINTENANCE
- EROSION CONTROL
- TREE/LIMB MAINTENANCE ON RIGHT-OF-WAY



CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN DRAINAGE

PROJECT NAME:
BEECHCREST DRIVE

FIGURE DR-02

LEGEND

- DRAINAGE PROJECT
- STREAMS
- WATERBODIES
- ROADS
- 1** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY
- 2013 RECOMMENDED DRAINAGE PROJECTS**
- 10 BEECHCREST

DRAINAGE COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- COMPLAINT REGARDING DRAINAGE PROJECT
- CURB/GUTTER MAINTENANCE
- DITCH MAINTENANCE
- DRAINAGE MAINTENANCE
- EROSION CONTROL
- TREE/LIMB MAINTENANCE ON RIGHT-OF-WAY



CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN DRAINAGE

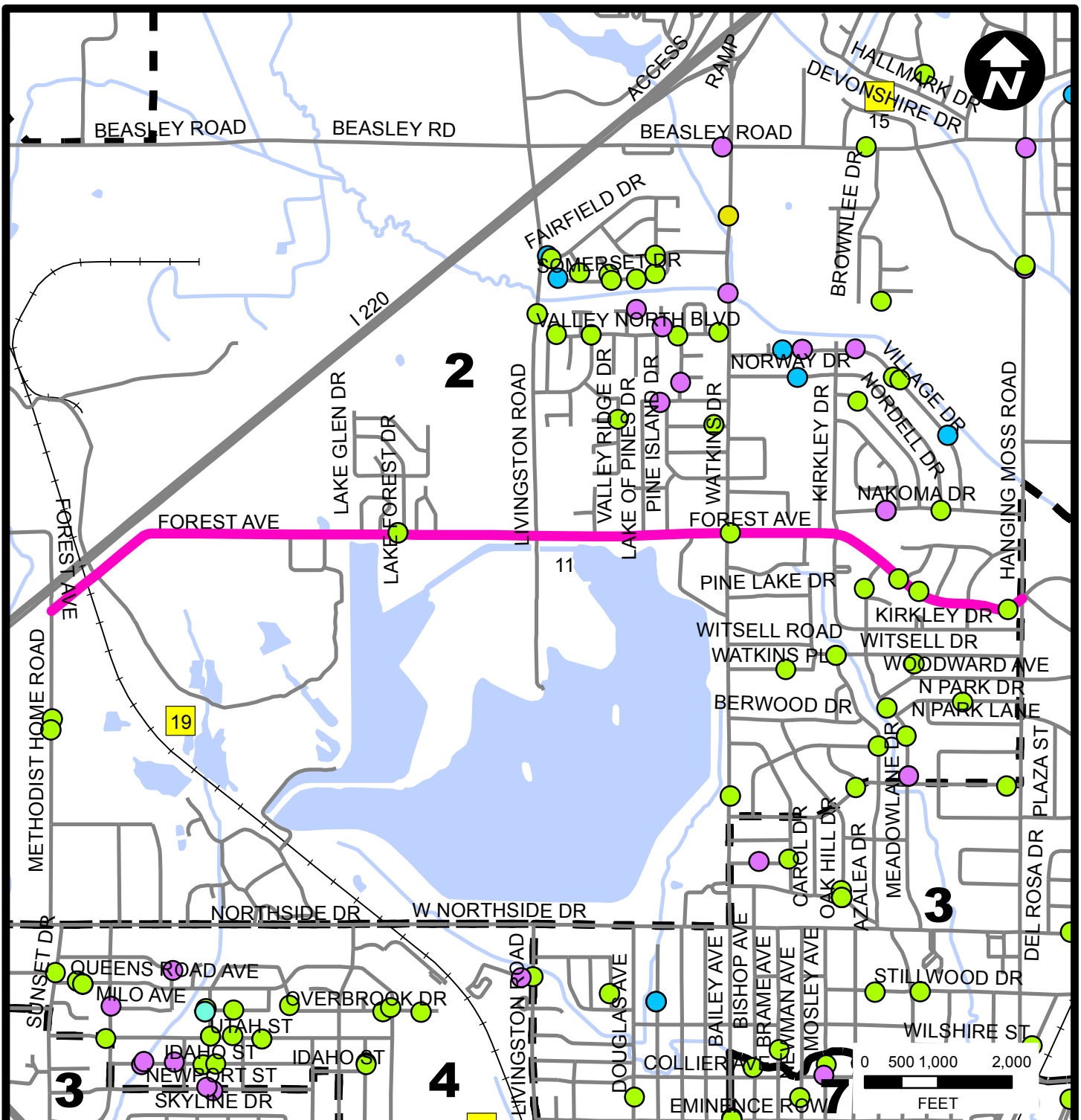
PROJECT NAME:
PARKWAY DR. TO AVENUE D
(GROVE PARK)
FIGURE DR-03

LEGEND

- DRAINAGE PROJECT
- STREAMS
- WATERBODIES
- ROADS
- 1** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY

DRAINAGE COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- COMPLAINT REGARDING DRAINAGE PROJECT
- CURB/GUTTER MAINTENANCE
- DITCH MAINTENANCE
- DRAINAGE MAINTENANCE
- EROSION CONTROL
- TREE/LIMB MAINTENANCE ON RIGHT-OF-WAY



CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN DRAINAGE

PROJECT NAME:
FOREST AVE. TO NORTHSIDE DR.
FIGURE DR-04

LEGEND

- DRAINAGE PROJECT
- STREAMS
- WATERBODIES
- ROADS

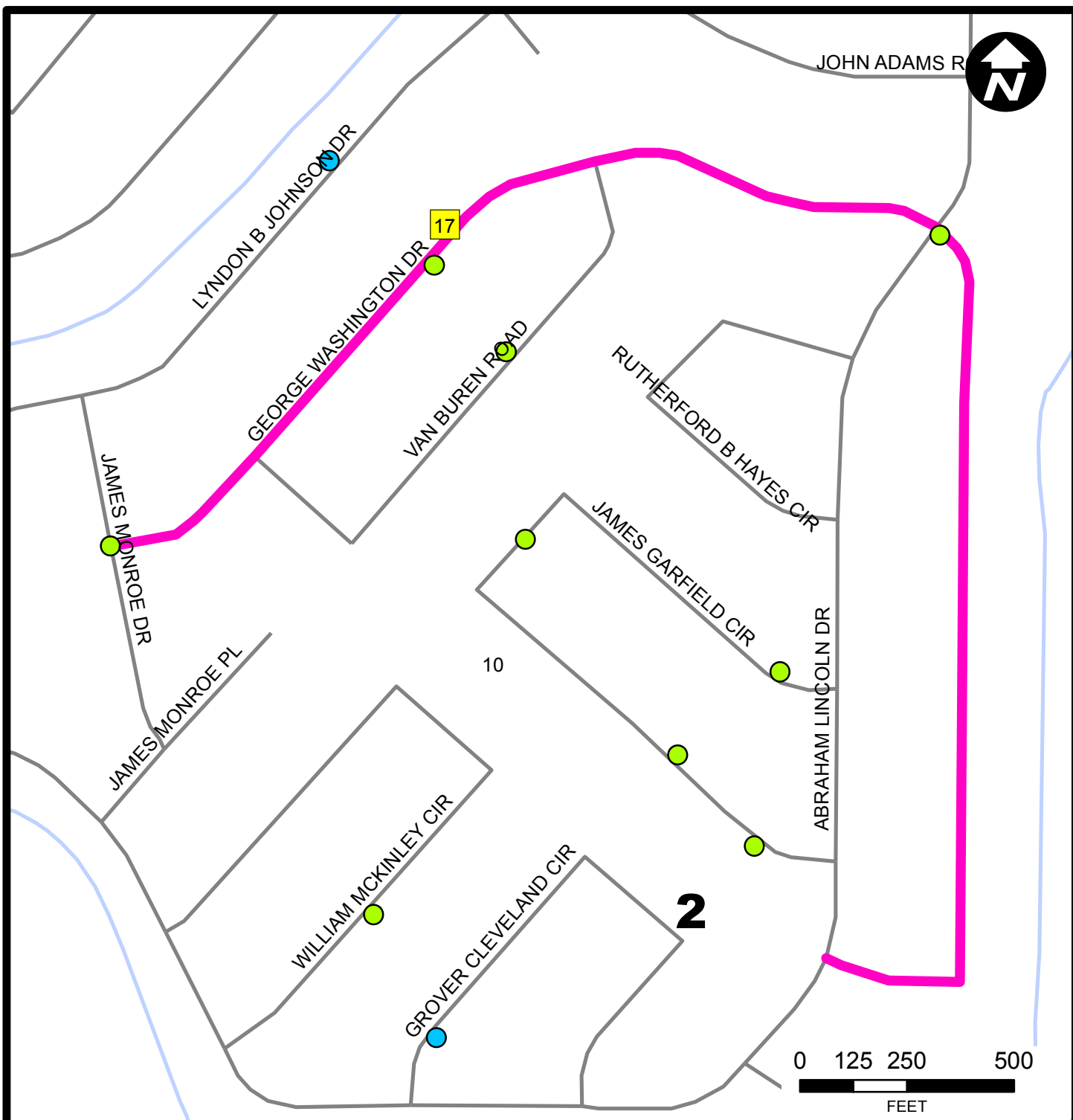
- 1** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY

2013 RECOMMENDED DRAINAGE PROJECTS

- 15 FONTAINE PARK/HALLMARK DR.
- 19 FOREST AVE. TO NORTHSIDE DR.

DRAINAGE COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- COMPLAINT REGARDING DRAINAGE PROJECT
- CURB/GUTTER MAINTENANCE
- DITCH MAINTENANCE
- DRAINAGE MAINTENANCE
- EROSION CONTROL
- TREE/LIMB MAINTENANCE ON RIGHT-OF-WAY



CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN DRAINAGE

PROJECT NAME:
GEORGE WASHINGTON DR

FIGURE DR-05

LEGEND

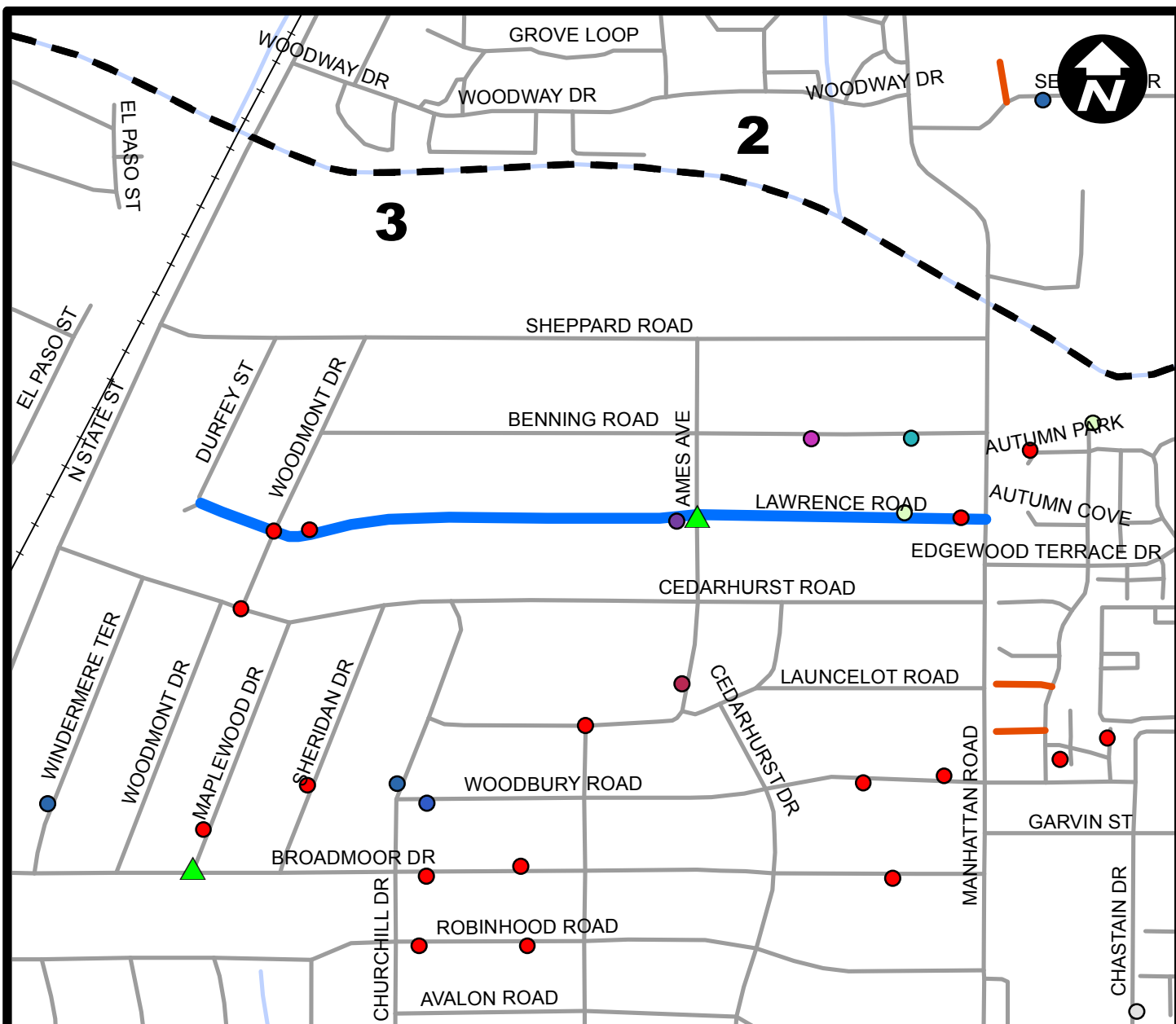
- DRAINAGE PROJECT
- STREAMS
- WATERBODIES
- ROADS
- 1** JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY

2013 RECOMMENDED DRAINAGE PROJECTS

- 17 GEORGE WASHINGTON DR.

DRAINAGE COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- COMPLAINT REGARDING DRAINAGE PROJECT
- CURB/GUTTER MAINTENANCE
- DITCH MAINTENANCE
- DRAINAGE MAINTENANCE
- EROSION CONTROL
- TREE/LIMB MAINTENANCE ON RIGHT-OF-WAY

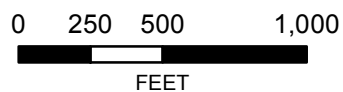


CITY OF JACKSON MISSISSIPPI

CAPITAL IMPROVEMENT PLAN WATER

PROJECT NAME:
LAWRENCE ROAD

FIGURE WTR-01



LEGEND

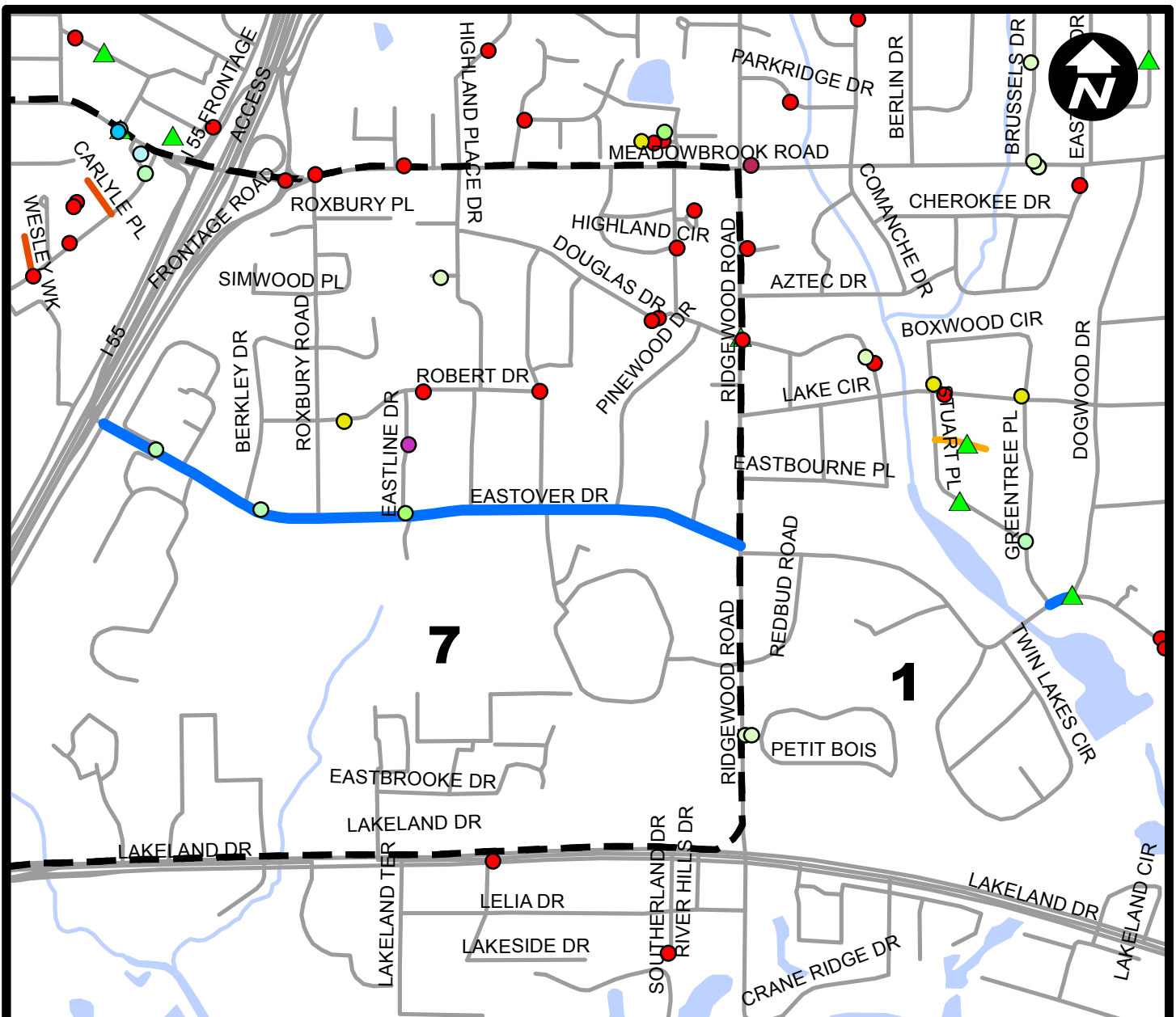
WATER COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- CURB STOP (TURN-OFF VALVE) BROKEN/STUCK
- DRAIN/COVER SWIMMING POOL
- FIRE HYDRANT - IN SERVICE
- FIRE HYDRANT - KNOCKED DOWN/HIT
- FIRE HYDRANT - LEAKING
- FIRE HYDRANT - OUT OF SERVICE
- FIRE HYDRANT - REQUEST TO CLOSE
- FIRE HYDRANT - REQUEST TO OPEN
- FIRE LINE TAP
- HIT SERVICE LINE
- INSTALLING COMMERCIAL TAP
- INSTALLING RESIDENTIAL TAP
- LEAK ON SERVICE
- METER - LEAKING
- METER BOX - LOWER / RAISE
- PULLED SERVICE
- PULLED TIE-IN
- RAISE FIRE HYDRANT
- RENEW SERVICE
- REPLACE WATER METER TOP

- ROADWAY SPILLS
- TURN OFF SERVICE FOR REPAIRS
- VALVE LEAKING
- WATER - CUSTOMER SHOULD HAVE WATER
- WATER - DISCOLORED
- WATER - LEAKING
- WATER - PRESSURE LOW
- WATER - TAG CUSTOMER'S DOOR
- WATER MAINTENANCE STREET RESTORATION
- WATER MAINTENANCE YARD RESTORATION
- WATER METER - CHANGE OUT
- BOIL WATER NOTIFICATIONS

- 2" WATER LINE
- 4" WATER LINE

- WATER PROJECT
- STREAMS
- WATERBODIES
- ROADS
- JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY



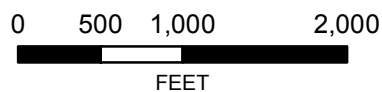
CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN WATER

PROJECT NAME:
EASTOVER DRIVE 1

FIGURE WTR-02



LEGEND

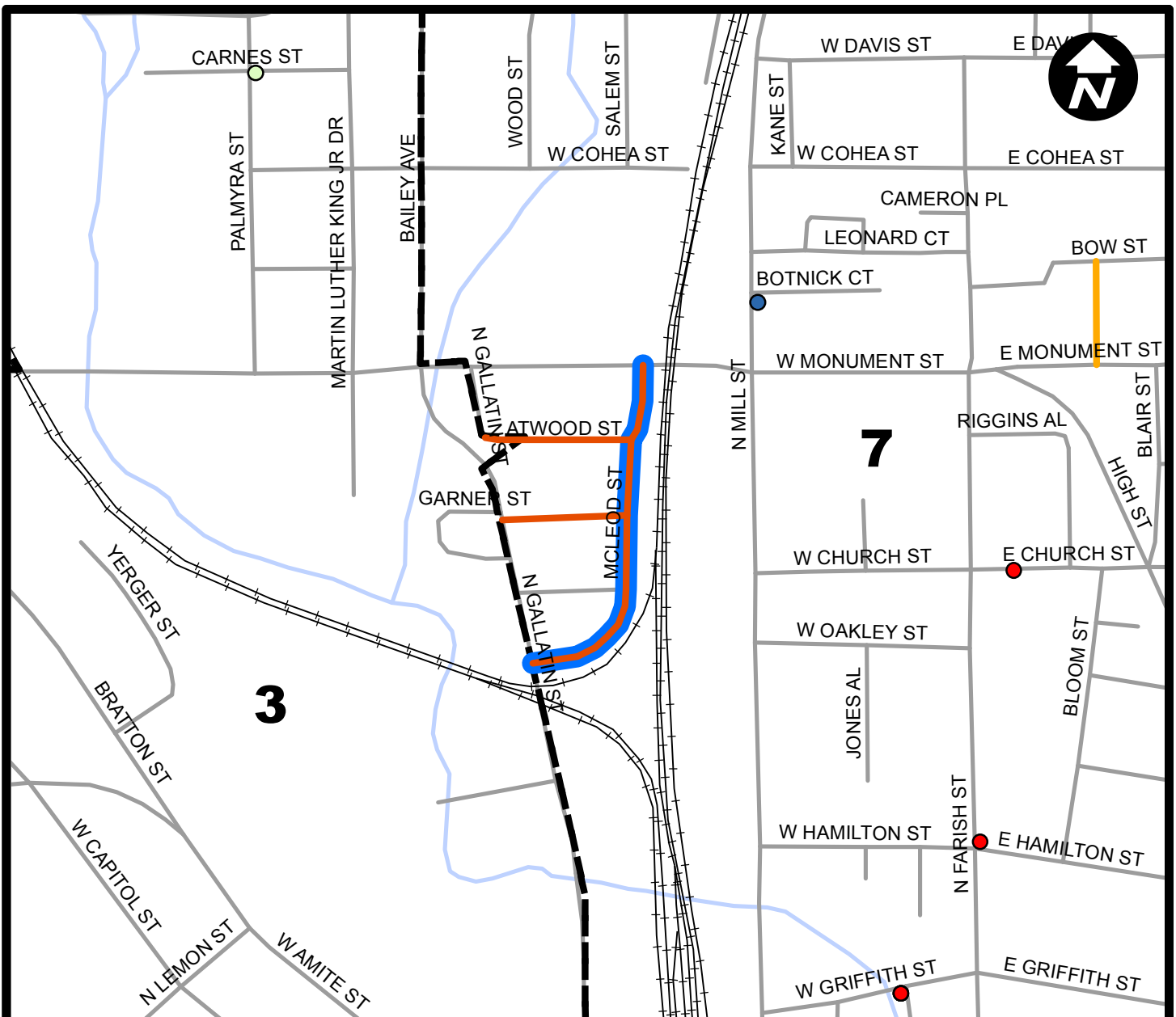
WATER COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- CURB STOP (TURN-OFF VALVE) BROKEN/STUCK
- DRAIN/COVER SWIMMING POOL
- FIRE HYDRANT - IN SERVICE
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- FIRE HYDRANT - LEAKING
- FIRE HYDRANT - OUT OF SERVICE
- FIRE HYDRANT - REQUEST TO CLOSE
- FIRE HYDRANT - REQUEST TO OPEN
- FIRE LINE TAP
- HIT SERVICE LINE
- INSTALLING COMMERCIAL TAP
- INSTALLING RESIDENTIAL TAP
- LEAK ON SERVICE
- METER - LEAKING
- METER BOX - LOWER / RAISE
- PULLED SERVICE
- PULLED TIE-IN
- RAISE FIRE HYDRANT
- RENEW SERVICE
- REPLACE WATER METER TOP

- ROADWAY SPILLS
- TURN OFF SERVICE FOR REPAIRS
- VALVE LEAKING
- WATER - CUSTOMER SHOULD HAVE WATER
- WATER - DISCOLORED
- WATER - LEAKING
- WATER - PRESSURE LOW
- WATER - TAG CUSTOMER'S DOOR
- WATER MAINTENANCE STREET RESTORATION
- WATER MAINTENANCE YARD RESTORATION
- WATER METER - CHANGE OUT
- BOIL WATER NOTIFICATIONS

- 2" WATER LINE
- 4" WATER LINE

- WATER PROJECT
- STREAMS
- WATERBODIES
- ROADS
- JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY



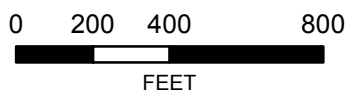
CITY OF JACKSON

MISSISSIPPI

CAPITAL IMPROVEMENT PLAN WATER

PROJECT NAME:
MCLEOD STREET

FIGURE WTR-03



LEGEND

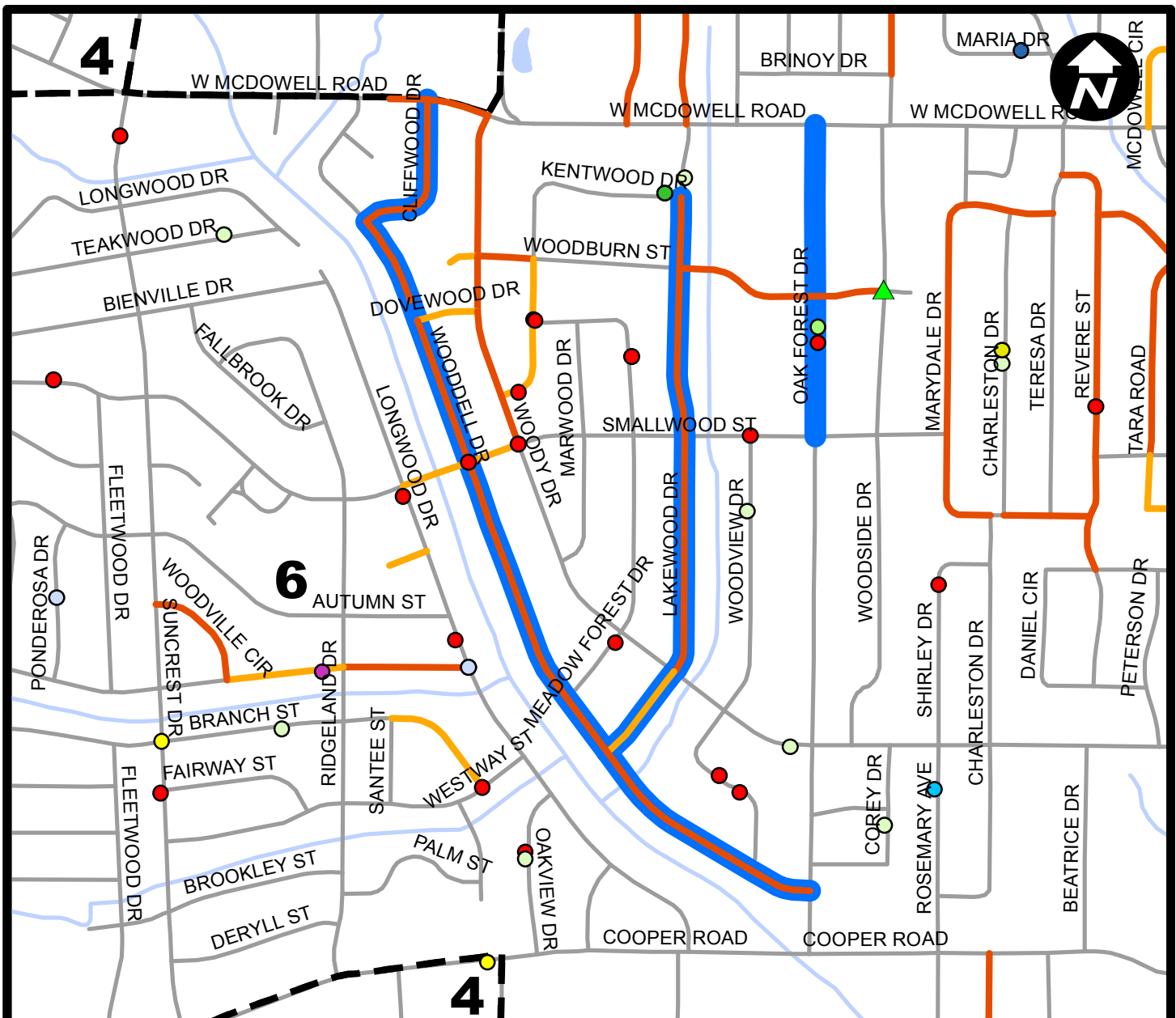
WATER COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- CURB STOP (TURN-OFF VALVE) BROKEN/STUCK
- DRAIN/COVER SWIMMING POOL
- FIRE HYDRANT - IN SERVICE
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- INSTALLING RESIDENTIAL TAP
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- METER - LEAKING
- METER BOX - LOWER / RAISE
- PULLED SERVICE
- PULLED TIE-IN
- RAISE FIRE HYDRANT
- RENEW SERVICE
- REPLACE WATER METER TOP

- ROADWAY SPILLS
- TURN OFF SERVICE FOR REPAIRS
- VALVE LEAKING
- WATER - CUSTOMER SHOULD HAVE WATER
- WATER - DISCOLORED
- WATER - LEAKING
- WATER - PRESSURE LOW
- WATER - TAG CUSTOMER'S DOOR
- WATER MAINTENANCE STREET RESTORATION
- WATER MAINTENANCE YARD RESTORATION
- WATER METER - CHANGE OUT
- BOIL WATER NOTIFICATIONS

- 2" WATER LINE
- 4" WATER LINE

- WATER PROJECT
- STREAMS
- WATERBODIES
- ROADS
- JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY



CITY OF JACKSON MISSISSIPPI

CAPITAL IMPROVEMENT PLAN WATER

PROJECT NAME:
WODELL DRIVE

FIGURE WTR-04

0 250 500 1,000
FEET

LEGEND

WATER COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- CURB STOP (TURN-OFF VALVE) BROKEN/STUCK
- DRAIN/COVER SWIMMING POOL
- FIRE HYDRANT - IN SERVICE
- FIRE HYDRANT - KNOCKED DOWN/HIT
- FIRE HYDRANT - LEAKING
- FIRE HYDRANT - OUT OF SERVICE
- FIRE HYDRANT - REQUEST TO CLOSE
- FIRE HYDRANT - REQUEST TO OPEN
- FIRE LINE TAP
- HIT SERVICE LINE
- INSTALLING COMMERCIAL TAP
- INSTALLING RESIDENTIAL TAP
- LEAK ON SERVICE
- METER - LEAKING
- METER BOX - LOWER / RAISE
- PULLED SERVICE
- PULLED TIE-IN
- RAISE FIRE HYDRANT
- RENEW SERVICE
- REPLACE WATER METER TOP

- ROADWAY SPILLS
- TURN OFF SERVICE FOR REPAIRS
- VALVE LEAKING
- WATER - CUSTOMER SHOULD HAVE WATER
- WATER - DISCOLORED
- WATER - LEAKING
- WATER - PRESSURE LOW
- WATER - TAG CUSTOMER'S DOOR
- WATER MAINTENANCE STREET RESTORATION
- WATER MAINTENANCE YARD RESTORATION
- WATER METER - CHANGE OUT
- BOIL WATER NOTIFICATIONS

- 2" WATER LINE
- 4" WATER LINE

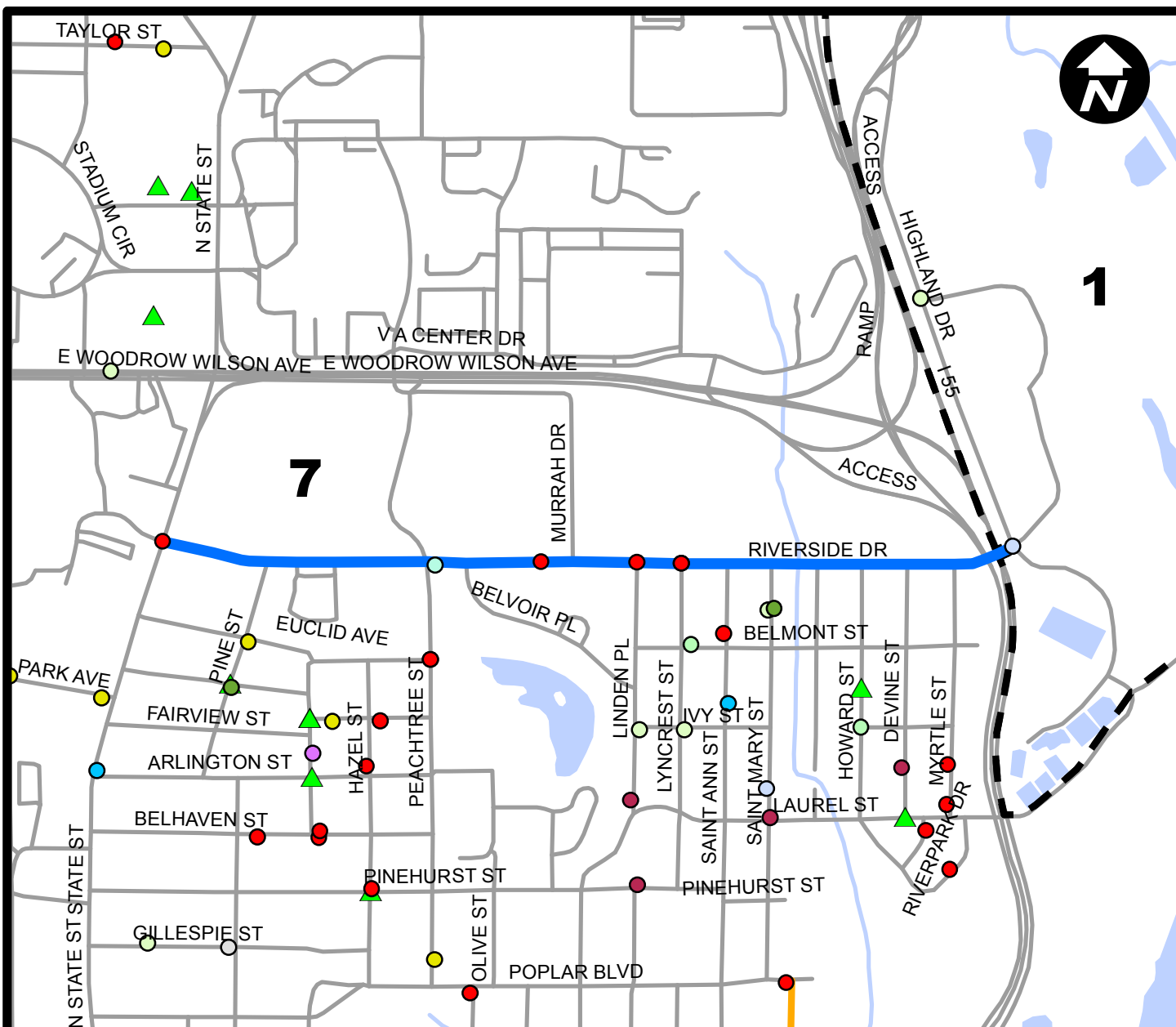
- WATER PROJECT

- STREAMS
- WATERBODIES

- ROADS

- JACKSON WARD NUMBER

- JACKSON WARD BOUNDARY



CITY OF JACKSON MISSISSIPPI

CAPITAL IMPROVEMENT PLAN WATER

PROJECT NAME:
RIVERSIDE DRIVE

FIGURE WTR-05

0 250 500 1,000
FEET

LEGEND

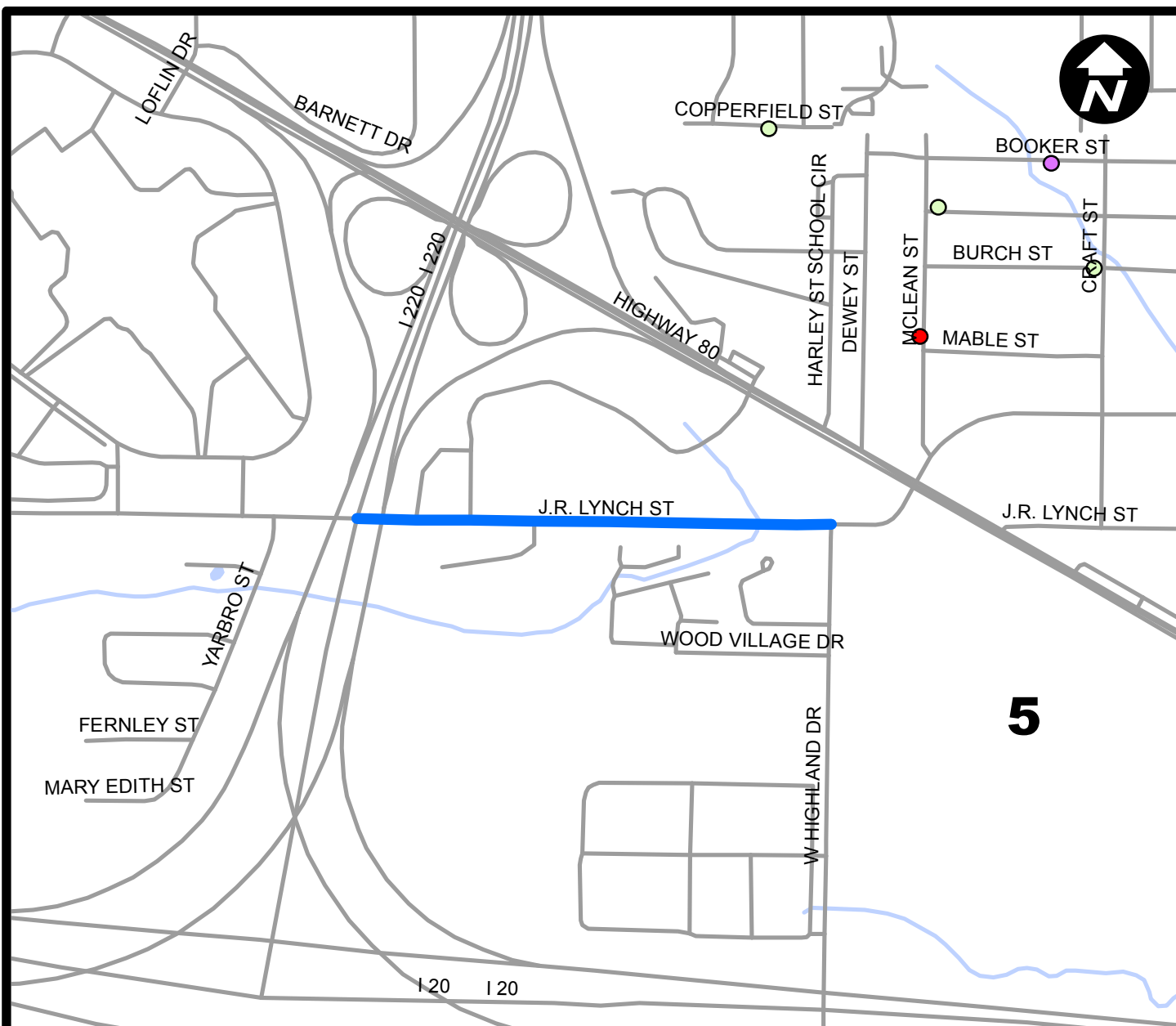
WATER COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- CURB STOP (TURN-OFF VALVE) BROKEN/STUCK
- DRAIN/COVER SWIMMING POOL
- FIRE HYDRANT - IN SERVICE
- FIRE HYDRANT - KNOCKED DOWN/HIT
- FIRE HYDRANT - LEAKING
- FIRE HYDRANT - OUT OF SERVICE
- FIRE HYDRANT - REQUEST TO CLOSE
- FIRE HYDRANT - REQUEST TO OPEN
- FIRE LINE TAP
- HIT SERVICE LINE
- INSTALLING COMMERCIAL TAP
- INSTALLING RESIDENTIAL TAP
- LEAK ON SERVICE
- METER - LEAKING
- METER BOX - LOWER / RAISE
- PULLED SERVICE
- PULLED TIE-IN
- RAISE FIRE HYDRANT
- RENEW SERVICE
- REPLACE WATER METER TOP

- ROADWAY SPILLS
- TURN OFF SERVICE FOR REPAIRS
- VALVE LEAKING
- WATER - CUSTOMER SHOULD HAVE WATER
- WATER - DISCOLORED
- WATER - LEAKING
- WATER - PRESSURE LOW
- WATER - TAG CUSTOMER'S DOOR
- WATER MAINTENANCE STREET RESTORATION
- WATER MAINTENANCE YARD RESTORATION
- WATER METER - CHANGE OUT
- ▲ BOIL WATER NOTIFICATIONS

- 2" WATER LINE
- 4" WATER LINE

- WATER PROJECT
- STREAMS
- WATERBODIES
- ROADS
- 4 JACKSON WARD NUMBER
- JACKSON WARD BOUNDARY



CITY OF JACKSON MISSISSIPPI

CAPITAL IMPROVEMENT PLAN WATER

PROJECT NAME:
J. R. LYNCH STREET

FIGURE WTR-06

0 250 500 1,000
FEET

LEGEND

WATER COMPLAINTS AND MAINTENANCE SERVICE REQUESTS

- CURB STOP (TURN-OFF VALVE) BROKEN/STUCK
- DRAIN/COVER SWIMMING POOL
- FIRE HYDRANT - IN SERVICE
- FIRE HYDRANT - KNOCKED DOWN/HIT
- FIRE HYDRANT - LEAKING
- FIRE HYDRANT - OUT OF SERVICE
- FIRE HYDRANT - REQUEST TO CLOSE
- FIRE HYDRANT - REQUEST TO OPEN
- FIRE LINE TAP
- HIT SERVICE LINE
- INSTALLING COMMERCIAL TAP
- INSTALLING RESIDENTIAL TAP
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- METER - LEAKING
- METER BOX - LOWER / RAISE
- PULLED SERVICE
- PULLED TIE-IN
- RAISE FIRE HYDRANT
- RENEW SERVICE
- REPLACE WATER METER TOP

- ROADWAY SPILLS
- TURN OFF SERVICE FOR REPAIRS
- VALVE LEAKING
- WATER - CUSTOMER SHOULD HAVE WATER
- WATER - DISCOLORED
- WATER - LEAKING
- WATER - PRESSURE LOW
- WATER - TAG CUSTOMER'S DOOR
- WATER MAINTENANCE STREET RESTORATION
- WATER MAINTENANCE YARD RESTORATION
- WATER METER - CHANGE OUT

▲ BOIL WATER NOTIFICATIONS

— 2" WATER LINE
— 4" WATER LINE

— WATER PROJECT
— STREAMS
— WATERBODIES
— ROADS
4 JACKSON WARD NUMBER
— JACKSON WARD BOUNDARY